

# *Individual Station Report*

## Westport

U R B I T R A N **R** E P O R T

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Prepared to

Connecticut Department of Transportation

Submitted by

Urbitran Associates, Inc.

July 2003

# *Stakeholder Interview*

U R B I T R A N **R** E P O R T



Prepared to

Connecticut Department of Transportation

Submitted by

Urbitran Associates, Inc.

## *Westport*

According to those at the meeting, which included the First and Second Selectmen and a representative from the Police Department, who run the station, Harry Harris wants CDOT to take control of the stations and parking. This was the first issue brought up by the town representatives – that the State wants to run the stations to provide better quality control, and that the State feels that this is the only solution to improve the supply of parking along the entire line. Furthermore, the feeling was that CDOT would be exempt from local zoning and would therefore be in a position to deck parking lots without local permission.

Westport feels that they do a good job with the two town stations, and that they have an excellent relationship with Carl Rosa regarding maintenance and operations and with Harry Harris regarding policy. They feel strongly that if other towns ran their stations and parking like they do CDOT would have far fewer issues to contend with. Westport understands the desire for uniformity among the stations and supports that policy, albeit with concern regarding home rule issues. Westport, ultimately, is satisfied with the status quo, and feels the working relationship is excellent, the division of responsibilities clear, and their ability to have input into the ADA design process excellent. They feel that the rail group at CDOT does not get enough money to carry out their mission. There were some concerns about the maintenance of the rail bridges and the retaining wall, and they wanted to ensure that their conditions were properly studied.

They just signed a new lease with CDOT in the past year, and clarified the financial record keeping so that there is a better mechanism for separating funds.

Westport had a simple message — we are happy to share control but are adamantly against relinquishing control. Local care and responsibility should come first for rail station users. At the same time, they raised a concern about the fees charged for parking at other stations, questioning why the charges are so high when it doesn't take a lot of money to properly operate and maintain a station/parking area. They feel that some towns are “milking the cow”, and that they are not necessarily serving regional transportation objectives but rather their own town financial interests. Westport does not believe in uniform fees unless they are low; furthermore, they feel that in most cases fees are not the barrier to rail use. The free lot a Bridgeport helped relieve the pressure on demand in surrounding towns and even they felt the impact. The town is fine with non-residents using the lots. The town position on parking is to have no more blacktop, and instead to encourage the use of the shuttles and park and ride lot.

# *Customer Opinion Survey*

U R B I T R A N **R** E P O R T



Prepared to

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Urbitran Associates, Inc.

## **Westport**

No surveys were distributed because the station was scheduled for construction for ADA rehabilitation. Therefore, customer opinions would have been significantly different; they would have reflected the situation prior to the major reconstruction and not the situation expected after the completion of the construction.

# *Parking Inventory and Utilization*

U R B I T R A N **R** E P O R T



Prepared to

Connecticut Department of Transportation

Submitted by

Urbitran Associates, Inc.

## Westport

Westport Station has 1,454 parking spaces distributed over eight station lots and a Park & Ride Lot on Imperial Avenue. Shuttle service is available for commuters who park at the Park & Ride Lot for a one-way fare of \$1.00.

Of the 1,454 parking spaces, 1,158 are permit spaces, 284 are for daily parking, and 12 spaces are designated as handicap. Lots 2, 3, and 5 were at capacity the day vehicle counts were taken. Lot 1 was near capacity at 97.6% utilization and all daily spaces were filled. Lots 4, 8, and the Imperial Park & Ride Lot were the least utilized. Lot 4 had 71.6% occupancy, Lot 8 had 47.1% occupancy and the Park and Ride Lot was only 20% occupied. Lots 4 and 8 are located across from the rail station on Saugatuck Avenue. The Imperial Avenue Park & Ride Lot is a longer distance away and requires a long walk or short shuttle ride to the station. In all, designated rail station parking was more than 85% used looking at all locations. The parking capacity and utilization are presented in detail in Table 11.

### *Parking Area Ownership*

Lot 1 is owned by the City of Westport while all other lots are owned by the State of Connecticut. The State owns 77.4% of the commuter parking at the Westport Station. Parking lot location and ownership status is outlined in Figure 11.

### *Fee Structure*

Cost to park at the Westport Station includes an annual fee of \$175 and a daily fee of \$4.00. Permits are good at both the Green's Farms and the Westport Stations. There are 1,700 people on the combined Green's Farms/Westport waiting list. Estimated wait time for a space is 3 to 5 years. This waitlist exists despite the fact that the Green's Farms/Westport Stations sell 3,300 permits annually, an over-sale rate of more than 100%.

A parking fee collection envelope is placed on the windshields of daily-parked cars. Parking is free at the Imperial Lot.

**Table 11: Westport Rail Station Parking Capacity and Utilization**

Location	Capacity	Vehicle Count	Utilization	Ownership
<b>Lot 1</b>				
Permit	312	304	97.4%	municipality
Daily	16	16	100.0%	
Handicap	0	0	N/A	
<b>Total Lot 1</b>	<b>328</b>	<b>320</b>	<b>97.6%</b>	
<b>Lot 2*</b>				
Permit	154	154	100.0%	state
Daily	16	16	100.0%	
Handicap	2	2	100.0%	
<b>Total Lot 2</b>	<b>172</b>	<b>172</b>	<b>100.0%</b>	
<b>Lot 3**</b>				
Permit	425	425	100.0%	state
Daily	82	82	100.0%	
Handicap	10	10	100.0%	
<b>Total Lot 3</b>	<b>517</b>	<b>517</b>	<b>100.0%</b>	
<b>Lot 4</b>				
Permit	59	29	49.2%	state
Daily	50	49	98.0%	
Handicap	0	0	N/A	
<b>Total Lot 4</b>	<b>109</b>	<b>78</b>	<b>71.6%</b>	
<b>Lot 5</b>				
Permit	63	63	100.0%	state
Daily	0	0	N/A	
Handicap	0	0	N/A	
<b>Total Lot 5</b>	<b>63</b>	<b>63</b>	<b>100.0%</b>	
<b>Lot 7</b>				
Permit	29	24	82.8%	state
Daily	0	0	N/A	
Handicap	0	0	N/A	
<b>Total Lot 7</b>	<b>29</b>	<b>24</b>	<b>82.8%</b>	
<b>Lot 8***</b>				
Permit	116	43	37.1%	state
Daily	20	21	105.0%	
Handicap	0	0	N/A	
<b>Total Lot 8</b>	<b>136</b>	<b>64</b>	<b>47.1%</b>	
<b>Park &amp; Ride</b>				
Permit	0	0	N/A	state
Daily	100	20	20.0%	
Handicap	0	0	N/A	
<b>Total Park &amp; Ride</b>	<b>100</b>	<b>20</b>	<b>20.0%</b>	
<b>Permit</b>	<b>1158</b>	<b>1042</b>	<b>90.0%</b>	<i>state</i>
<b>Daily</b>	<b>284</b>	<b>204</b>	<b>71.8%</b>	<b>1126</b>
<b>Handicap</b>	<b>12</b>	<b>12</b>	<b>100.0%</b>	<i>municipality</i>
<b>TOTAL PARKING</b>	<b>1454</b>	<b>1258</b>	<b>86.5%</b>	<b>328</b>

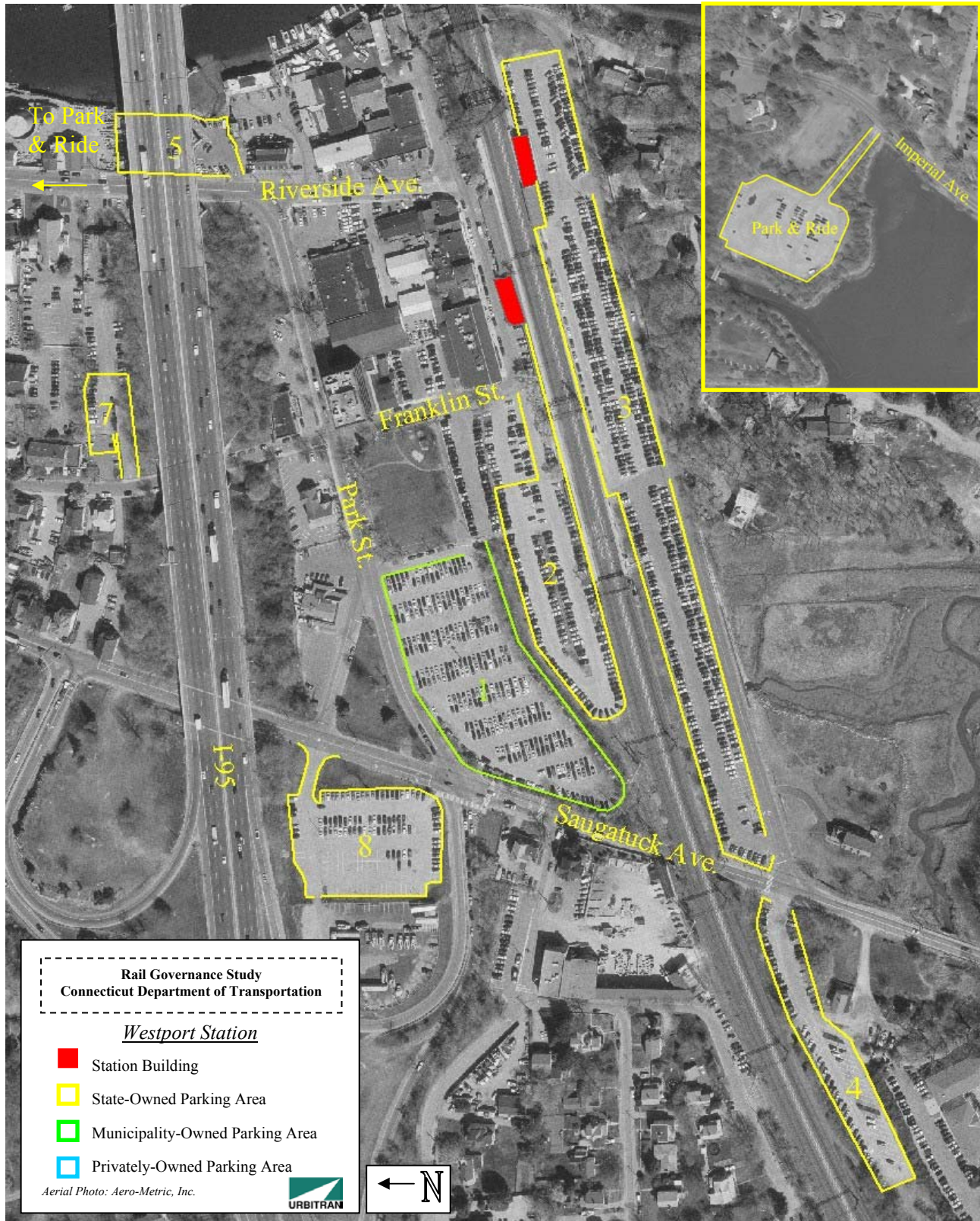
\* Daily' represents space parking after 9:15 AM.

\*\*Lot 3 also contains an additional 40 spaces designated for Avis and taxis.

\*\*\*Town inventory shows 2 spaces designated for handicap but there was no signage indicating 'Handicap Only.' Spaces in use exceed capacity for daily parking due to 1 car parked illegally.



Figure 11: Westport Rail Station Parking Map



# *Station Condition Inspection*

U R B I T R A N **R** E P O R T



Prepared to

Connecticut Department of Transportation

Submitted by

Urbitran Associates, Inc.

CONNECTICUT  
DEPARTMENT  
OF  
TRANSPORTATION



CONDITION INSPECTION  
FOR THE  
WESTPORT STATION

GENERAL RECOMMENDATION 2

**CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION**

**INSPECTION RATING SCALE**

*The following rating scale is used for inspections:*

- 1- Totally deteriorated, or in failed condition.
- 2- Serious deterioration, or not functioning as originally designed.
- 3- Minor deterioration, but functioning as originally designed.
- 4- New condition. No deterioration.
- 5- Not applicable.
- 6- Condition and/or existence unknown.

STATION: Westport  
 LINE: New Haven Line  
 INSPECTION DATE: 8/02/02  
 INSPECTION AGENCY / FIRM: UA  
 INSPECTORS: RGW  
 WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 1 OF 55

(Southbound) North Platform

SPAN NO.	PLATFORM ELEMENT										CANOPY				SUPER-STRUCTURE	FOUNDATIONS		
	1 RAILING	2 RAILING PAINT	3 STAIRS	4 JOINTS	5 TOP OF PLATFORM	6 BENCHES	7 SIGN / BILLBOARD	8 WARNING STRIP	9 PLATFORM EDGE RUBBING BOARD	10 PEDESTRIAN TUNNEL	11 COLUMNS OVERALL	12 COLUMN BASE @ PLATFORM	13 ROOF FRAMING ELEMENTS	14 ROOFING MATERIAL	15 DOUBLE TEE	16 PIER	17 FOOTING	18 EROSION / SCOUR
I	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
II	3	3	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
III	3	3	5	3	3	5	3	3	5	5	3	3	3	2	3	5	3	
IV	3	3	5	3	3	3	3	3	5	5	3	3	3	2	3	5	3	
V	3	3	5	3	3	3	3	3	5	5	3	3	3	2	3	5	3	
VI	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
VII	2	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
VIII	3	3	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
IX	3	2	5	3	3	5	5	3	5	5	5	5	5	3	5	3	3	
X	3	3	5	3	3	5	3	3	5	5	3	3	3	2	3	5	3	
XI	3	2	3	3	3	5	3	3	5	5	3	3	3	2	3	3	3	
XII	3	2	5	3	3	3	3	3	5	5	3	3	3	2	3	5	3	
XIII	3	3	5	3	3	5	3	3	5	5	3	3	3	2	3	5	3	
XIV	3	3	3	3	3	5	3	3	5	5	3	3	3	2	3	3	3	
XV	3	3	3	3	3	5	5	3	5	5	3	3	3	2	3	3	3	
XVI	3	3	5	3	3	3	3	3	5	5	3	3	3	2	3	5	3	
XVII	3	3	5	3	3	5	5	3	5	5	3	3	3	2	3	5	3	
XVIII	3	3	5	3	3	5	3	3	5	5	3	3	3	2	3	5	3	
XIX	3	3	3	3	3	5	3	3	5	5	3	3	3	2	3	3	3	
XX	3	3	3	3	3	5	3	3	5	5	3	3	3	2	3	3	3	
XXI	3	3	3	3	3	3	3	3	5	5	3	3	3	2	3	3	3	
XXII	3	3	5	3	3	5	3	3	5	3	5	5	5	5	3	5	3	
XXIII	3	3	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XXIV	3	3	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	

STATION: Westport  
 LINE: New Haven Line  
 INSPECTION DATE: 8/02/02  
 INSPECTION AGENCY / FIRM: UA  
 INSPECTORS: RGW  
 WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 2 OF 55

(Northbound) South Platform

SPAN NO.	PLATFORM ELEMENT										CANOPY				SUPER-STRUCTURE	FOUNDATIONS		
	1 RAILING	2 RAILING PAINT	3 STAIRS	4 JOINTS	5 TOP OF PLATFORM	6 BENCHES	7 SIGN / BILLBOARD	8 WARNING STRIP	9 PLATFORM EDGE RUBBING BOARD	10 PEDESTRIAN TUNNEL	11 COLUMNS OVERALL	12 COLUMN BASE @ PLATFORM	13 ROOF FRAMING ELEMENTS	14 ROOFING MATERIAL	15 DOUBLE TEE	16 PIER	17 FOOTING	18 EROSION / SCOUR
I	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
II	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
III	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
IV	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
V	3	3	5	3	3	3	3	3	5	5	5	5	5	3	5	3	3	
VI	3	3	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
VII	3	2	5	3	3	3	5	3	5	5	5	5	5	3	5	3	3	
VIII	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
IX	3	3	5	3	3	5	5	3	5	5	5	5	5	3	5	3	3	
X	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XI	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XII	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XIII	3	2	5	3	3	3	3	3	5	5	5	5	5	3	5	3	3	
XIV	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
XV	3	2	5	3	3	3	5	3	5	5	5	5	5	3	5	3	3	
XVI	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	
XVII	3	2	5	3	3	5	5	3	5	5	5	5	5	3	5	3	3	
XVIII	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XIX	3	2	5	3	2	5	3	3	5	5	3	3	2	3	5	3	3	
XX	3	2	3	3	3	5	3	3	5	5	5	3	2	3	3	3	3	
XXI	3	2	3	3	3	5	3	3	5	3	3	3	2	3	3	3	3	
XXII	3	3	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XXIII	3	2	5	3	3	5	3	3	5	5	5	5	5	3	5	3	3	
XXIV	3	2	3	3	3	5	3	3	5	5	5	5	5	3	3	3	3	

STATION: Westport  
LINE: New Haven Line  
INSPECTION DATE: 8/02/02  
INSPECTION AGENCY / FIRM: UA  
INSPECTORS: RGW  
WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 3 OF 55

### PARKING ELEMENTS

#### QUADRANT # I

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 2

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 2  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 2

CURB: 3

#### QUADRANT # II

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 3

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 5  
( FOR LOCATION SEE SHEET: \_\_\_\_\_ )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 3

CURB : 3

STATION: Westport  
LINE: New Haven Line  
INSPECTION DATE: 8/02/02  
INSPECTION AGENCY / FIRM: UA  
INSPECTORS: RGW  
WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 4 OF 55

### PARKING ELEMENTS

#### QUADRANT # III

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 3

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 2  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 3

CURB: 2

#### QUADRANT # IV

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 3

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 3  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 5

CURB : 3



STATION: Westport  
LINE: New Haven Line  
INSPECTION DATE: 8/02/02  
INSPECTION AGENCY / FIRM: UA  
INSPECTORS: RGW  
WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 5 OF 55

**PARKING ELEMENTS**

**QUADRANT # V**

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 3

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 3  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 3

CURB: 3

**QUADRANT # VI**

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 2

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 3  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 5

LANDSCAPE: 3

SIDEWALK: 5

CURB : 3

STATION: Westport  
LINE: New Haven Line  
INSPECTION DATE: 8/02/02  
INSPECTION AGENCY / FIRM: UA  
INSPECTORS: RGW  
WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 6 OF 55

**PARKING ELEMENTS**

**QUADRANT # VII**

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 2

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 5  
( FOR LOCATION SEE SHEET: \_\_\_\_\_ )

SIGNAGE: 3

FENCE AND GUARDRAIL: 5

LANDSCAPE: 3

SIDEWALK: 3

CURB: 2

**QUADRANT # VIII**

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 2

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 5  
( FOR LOCATION SEE SHEET: \_\_\_\_\_ )

SIGNAGE: 3

FENCE AND GUARDRAIL: 5

LANDSCAPE: 3

SIDEWALK: 5

CURB : 3

STATION: Westport  
LINE: New Haven Line  
INSPECTION DATE: 8/02/02  
INSPECTION AGENCY / FIRM: UA  
INSPECTORS: RGW  
WEATHER: Sunny, 80's

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 7 OF 55

### PARKING ELEMENTS

#### QUADRANT # IX

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 3

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 3  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 2

CURB: 2

#### QUADRANT # X

TYPE OF SURFACE: asphalt x PAVED; \_\_\_\_\_ GRAVEL; \_\_\_\_\_ DIRT;  
\_\_\_\_\_ OTHER (DESCRIBE)

CONDITION OF PAVED SURFACE: 2

CONDITION OF STRIPING: 3

CONDITION OF BASIN / DRAINS / ETC: 3  
( FOR LOCATION SEE SHEET: see sketch )

SIGNAGE: 3

FENCE AND GUARDRAIL: 3

LANDSCAPE: 3

SIDEWALK: 5

CURB : 3

STATION: Westport  
 LINE: New Haven  
 INSPECTION DATE : January 4, 2002  
 INSPECTION AGENCY / FIRM: Parsons Brinckerhoff  
 INSPECTORS: Jim Connell & Dave Lang  
 TIME OF INSPECTION: A.M.  
 WEATHER: Clear

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 8 OF 55

**PLATFORM --- LIGHTING**

Span Number	Fixture Type	Manufacturer	Model Number	Rating	Support Condition	Estimated Age/Life(y/y)	Visual Condition
all	HID-MH	unknown	unknown	1	1	17/ 20	totally deteriorated

Remarks: A typical section of the platform was measured at the location indicated and found to average 7.56 and 7.3 on the southbound and northbound platforms, respectively.  
The majority of the light pole bases have failed in the past and attempts have been made to repair them.  
Much of the lighting conduit system is corroded and should be replaced.

**PLATFORM --- LIGHTING LEVELS (fc)**

NORTHBOUND PLATFORM					
see remarks	see remarks	see remarks	see remarks	avg 7.3	see remarks

TRACKS----{ 

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see remarks	avg 7.56	see remarks	see remarks	see remarks	see remarks
-------------	-------------	-------------	-------------	-------------	-------------

SOUTHBOUND PLATFORM					
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STATION: Westport  
 LINE: New Haven  
 INSPECTION DATE : January 4, 2002  
 INSPECTION AGENCY / FIRM: Parsons Brinckerhoff  
 INSPECTORS: Jim Connell & Dave Lang  
 TIME OF INSPECTION: A.M.  
 WEATHER: Clear

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 9 OF 55

**PLATFORM --- SERVICE**

Voltage Rating (V)	120/240	Type of 3 phase connection		Delta	n/a	Wye	n/a
		Method of Entrance		Overhead	n/a	Underground	n/a
Rating of Main Breaker (A)	see remarks	Origin of Service		Pole	n/a	Transformer	n/a
		Code Compliant		Yes	n/a	No	n/a
Quantity of Phases	1	Pole Number & Street	see remarks	Wire Sizes	unknown		

Remarks: The southbound platform is serviced by branch circuits from the station building panelboard. The utility service to the station building is from pole CL&P 25673. The northbound platform is serviced by a separate electrical utility connection from pole CL&P, no number, this service terminates in an electrical enclosure located on the platform.

**PLATFORM --- ELECTRICAL SYSTEMS**

Electrical Device	Manufacturer	Model Number	Rating	Location	Estimated Age/Life(y/y)	Visual Condition
Main Distribution Panel	unknown	unknown	unknown	platform	unknown	unknown
Main Disconnect Switch	n/a	n/a	n/a	n/a	n/a	n/a
Transformer	n/a	n/a	n/a	n/a	n/a	n/a
Receptacles	unknown	unknown	1	platform	17/ 20	totally deteriorated
Grounding	unknown	unknown	unknown	unknown	unknown	unknown
Lighting Controls	unknown	unknown	unknown	unknown	unknown	unknown
Public Telephone	unknown	n/a	n/a	platform	n/a	operational
Station Telephone	n/a	n/a	n/a	n/a	n/a	n/a

Remarks: A majority of the receptacles need to be replaced with GFCI type receptacles.

STATION: Westport

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 10 OF 55

INSPECTORS: Jim Connell & Dave Lang

DATE: January 4, 2002

**STATION PLATFORM --- ELECTRICAL AND LIGHTING SUMMARY**

The electrical service to the southbound platform is derived from the building, while the electrical service to the northbound platform is derived from a separate underground source. A few non-GFCI receptacles are located on the platforms and some receptacles are missing their covers. We suggest that the receptacles be replaced with GFCI type with covers.

Both platforms have pole-mounted luminaires with the southbound platform maintaining an average of 7.56 foot-candles and the northbound platform averages 7.3 foot-candles. The platform lighting conduits and the bases of the poles have totally deteriorated. This deterioration may be caused by the heavy use of salt on the platforms during the winter months. We suggest that the lighting poles and conduits be replaced. The pedestrian tunnel beneath the train tracks maintains an average 9.47 foot-candles. Both platforms and the pedestrian tunnel exceed the minimum light levels recommended by the IESNA.

STATION: Westport  
 LINE: New Haven  
 INSPECTION DATE : January 23, 2002  
 INSPECTION AGENCY / FIRM: Parsons Brinckerhoff  
 INSPECTORS: J. Duncan & T. Abrahamson  
 TIME OF INSPECTION: A.M.  
 WEATHER: Clear & Cold

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 11 OF 55

**PLATFORM - PLUMBING**

SPAN NO.	GUTTER	DOWNSPOUT/ PIPING	CLEAN-OUTS	SPAN NO.	GUTTER	DOWNSPOUT/ PIPING	CLEAN-OUTS
	<u>Southbound Platform</u>						
All	Rain canopy gutters and downspouts are in good condition except one downspout is missing at the end of the platform and one lower part of a downspout is missing near the building .						
	At the far end of the platform there is another rain canopy. This is completely rusted, it has no gutters or downspouts.						
	<u>Northbound Platform</u>						
	There is a rain canopy/shelter, the aluminum gutters are in good condition, but there are no downspouts.						

**PLATFORM - FIXTURES--N/A**

SPAN #: \_\_\_\_\_  
 MODEL: \_\_\_\_\_  
 YEAR: \_\_\_\_\_  
 MANUFACTURER: \_\_\_\_\_  
 CONDITION: \_\_\_\_\_

SPAN #: \_\_\_\_\_  
 MODEL: \_\_\_\_\_  
 YEAR: \_\_\_\_\_  
 MANUFACTURER: \_\_\_\_\_  
 CONDITION: \_\_\_\_\_

SPAN #: \_\_\_\_\_  
 MODEL: \_\_\_\_\_  
 YEAR: \_\_\_\_\_  
 MANUFACTURER: \_\_\_\_\_  
 CONDITION: \_\_\_\_\_

STATION: Westport

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET 12 OF 55

INSPECTORS: J. Duncan, T. Abrahamson

DATE: January 23, 2002

**STATION – MECHANICAL SUMMARY**

The only task at the Westport station was to inspect the platform. On the southbound side of the platform the rain canopy gutters and downspouts are in good condition except for two missing downspouts. The location of one missing downspout is at the end of the building, the lower part of the downspout is missing. At the end of the platform there is one downspout, which is deteriorated. There is another rain canopy at the far end of the platform. This rain canopy is completely rusted, severely deficient and has no gutters or downspouts.

On the northbound platform there is a rain shelter, the gutters are aluminum and in good condition. There are no downspouts on the canopy.



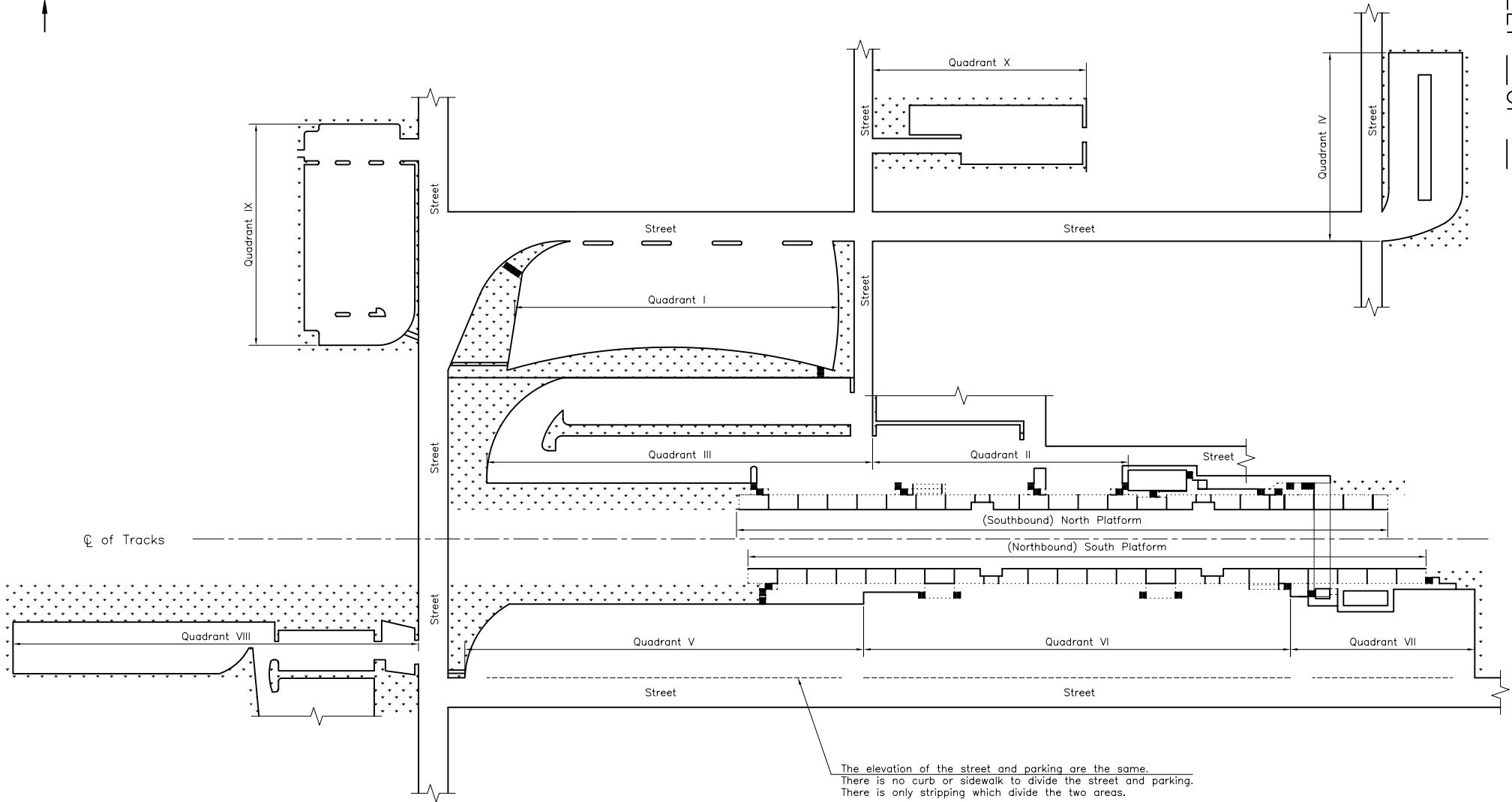
STATION: \_\_\_\_\_

CONN. DEPT OF TRANSPORTATION  
STATION INSPECTION REPORT  
SHEET \_\_\_\_\_ OF \_\_\_\_\_

INSPECTORS: \_\_\_\_\_

DATE: \_\_\_\_ / \_\_\_\_ / \_\_\_\_

SEE AUTOCAD FILE  
13-25

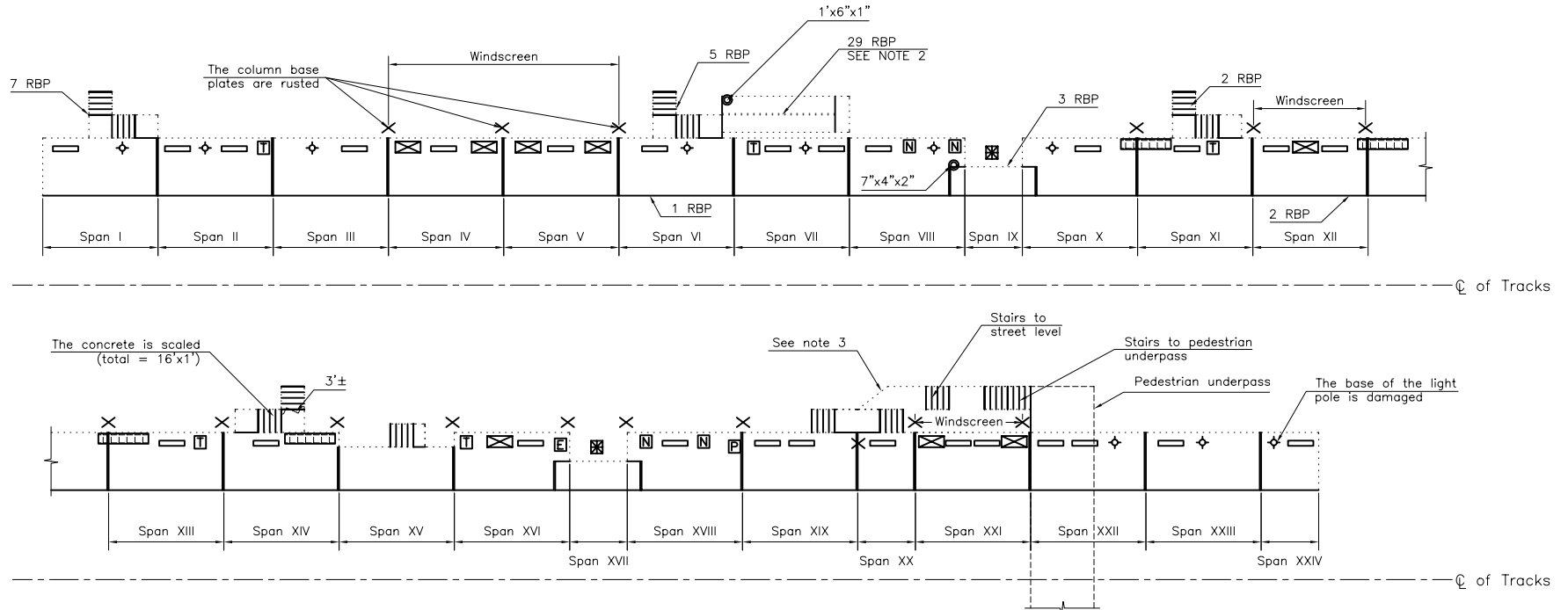


☉ of Tracks

Legend:

- ..... Pedestrian Rail
- ▣ Grass

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station General Plan
Date: 8/03/02



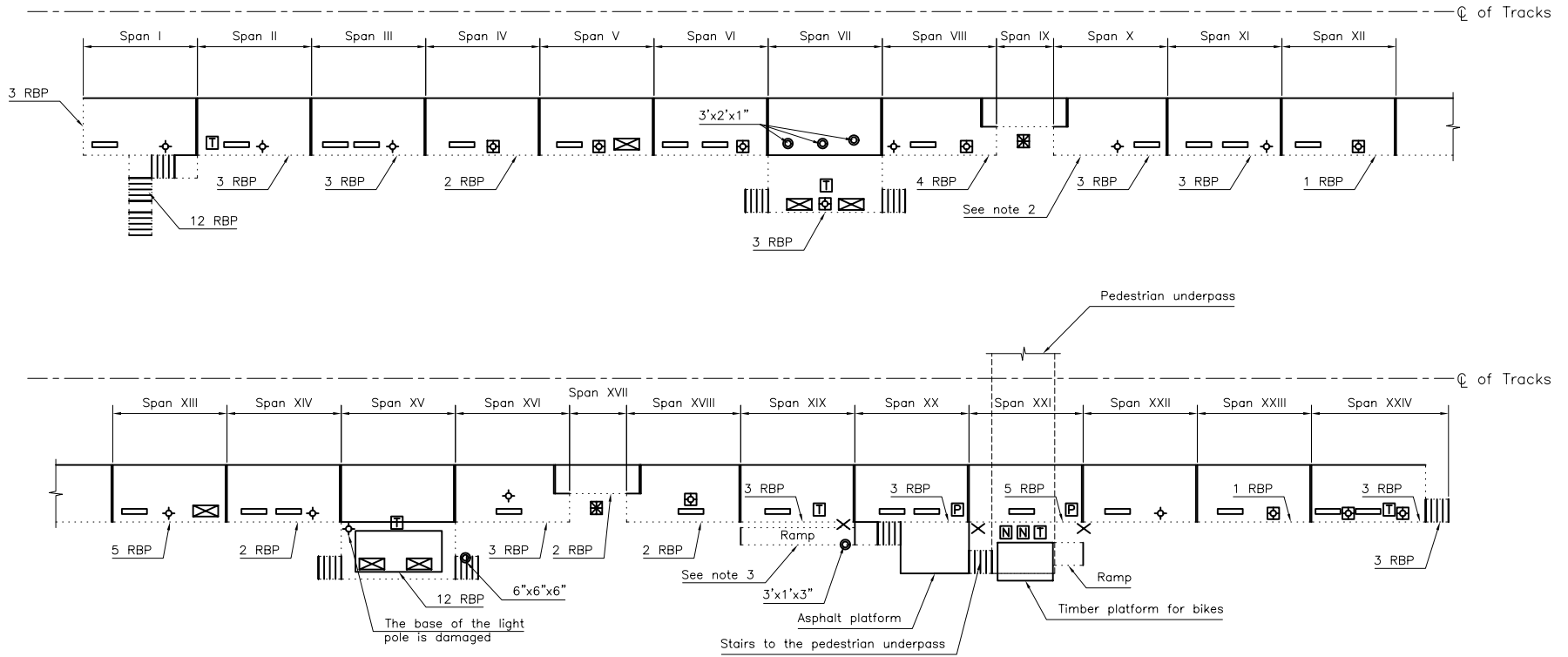
Legend:

- ..... Pedestrian Rail
- ~~~~~ Crack
- Spalled Concrete
- ◆ Light
- × Canopy Column
- ▭ Sign
- ⊠ Bench
- ⊞ Trash Receptacle
- ⊞ Newspaper Stand
- ⊞ Telephone
- ⊞ Electric Box
- Joint
- ⊞ Train Power Line
- ▭ Portable Metal Platform
- RBP Rusted Base Plate

NOTES:

1. Through out the platform the base plates for the double tee are rusted and deteriorated.
2. There are five locations where the railing post is damaged.
3. The concrete curb/retaining wall is spalled.
4. The paint on the canopy structure is chipping and peeling.

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station (Southbound) North Platform Plan
Date: 8/02/02



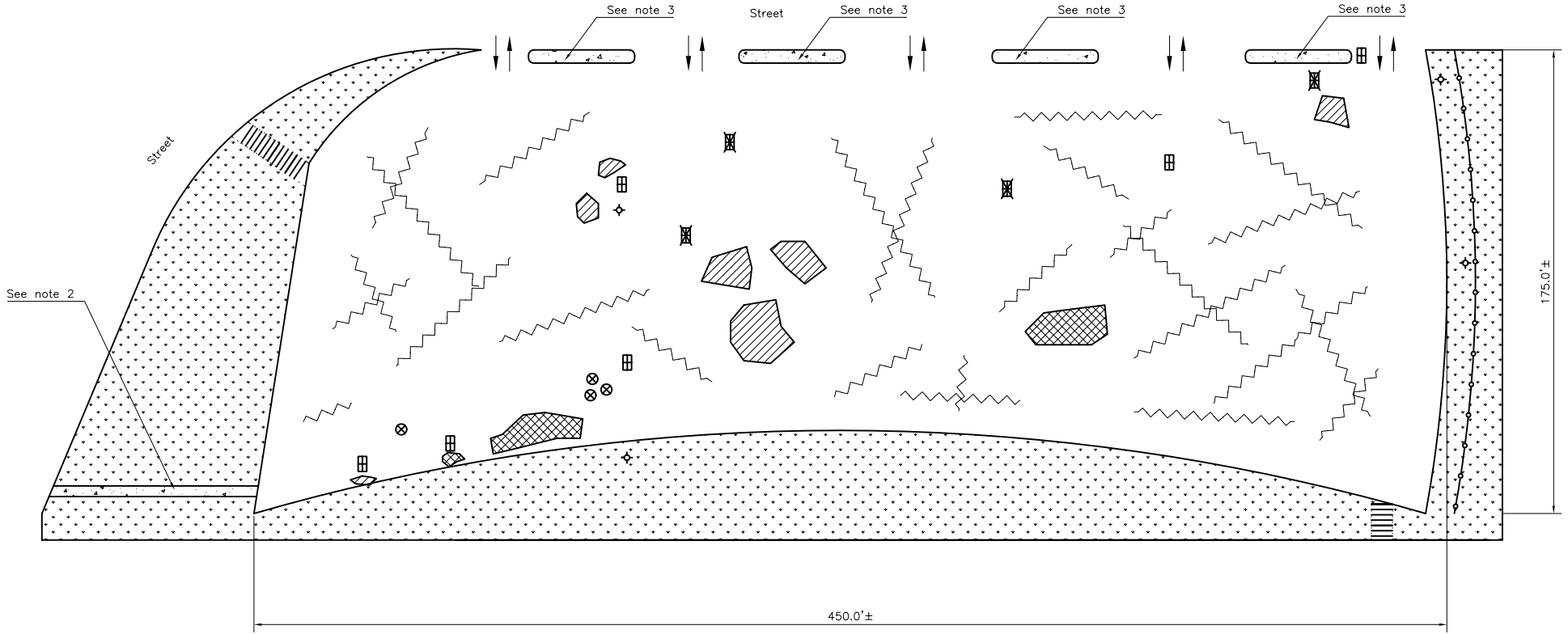
Legend:

- ..... Pedestrian Rail
- Spalled Concrete
- ⊕ Light
- ⊗ Rusted Light Pole Base
- × Canopy Column
- ▭ Sign
- ⊠ Bench
- ⊞ Trash Receptacle
- ⊞ Newspaper Stand
- ⊞ Telephone
- Joint
- ⊗ Train Power Line Pole
- RBP Rusted Base Plate

NOTES:

1. Through out the platform the base plates for the double tee are rusted and deteriorated.
2. There are two cut posts.
3. There are six locations of spalled concrete adjacent to the rusted and deteriorated railing base plate.
4. The paint on the canopy structure is chipping and peeling.

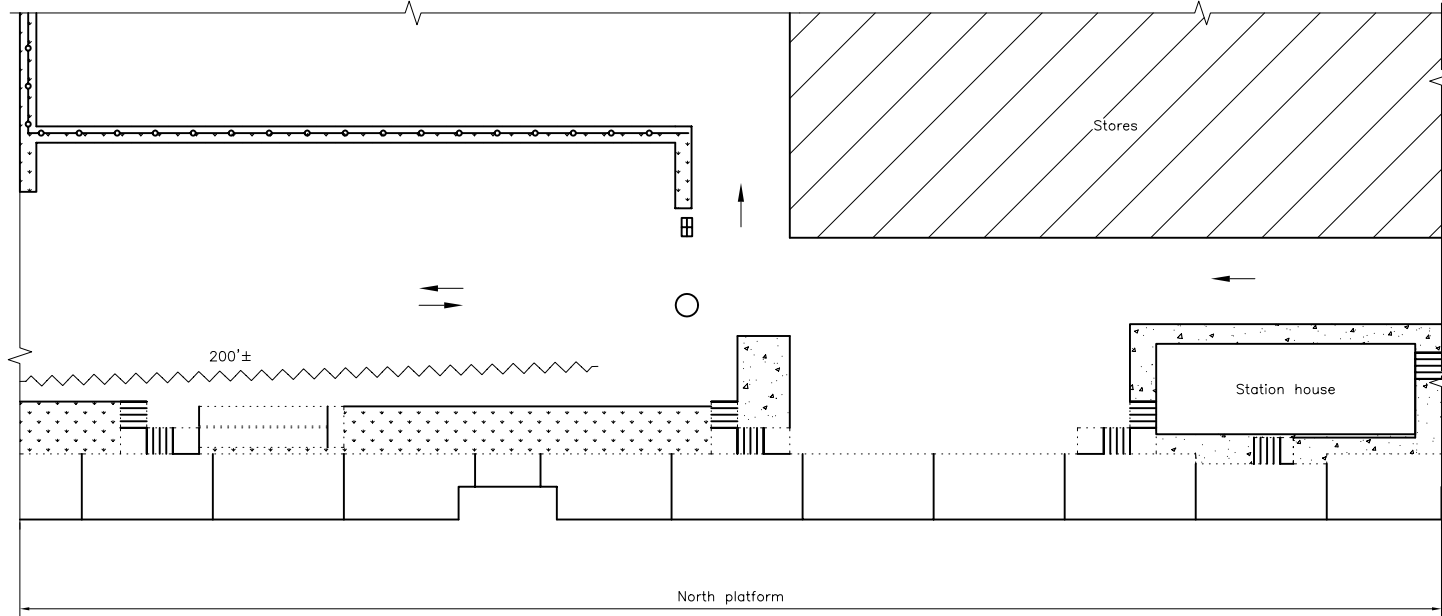
Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station (Northbound) South Platform Plan
Date: 8/02/02



- NOTES:
1. The entire asphalt surface contains numerous cracks with vegetation growth. There are multiple locations of potholes and an uneven asphalt surface where ponding of water was noted.
  2. The asphalt sidewalk is cracked with an uneven walking surface.
  3. There is vegetation growth from the asphalt sidewalk / median.

- Legend:
- Fence
  - Pedestrian Rail
  - Crack
  - Pothole with Ponding Water
  - Ponding Water
  - Grass
  - Sidewalk
  - Drain
  - Drain Covered with Vegetation
  - Pothole
  - Light

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant I Plan
Date: 8/03/02



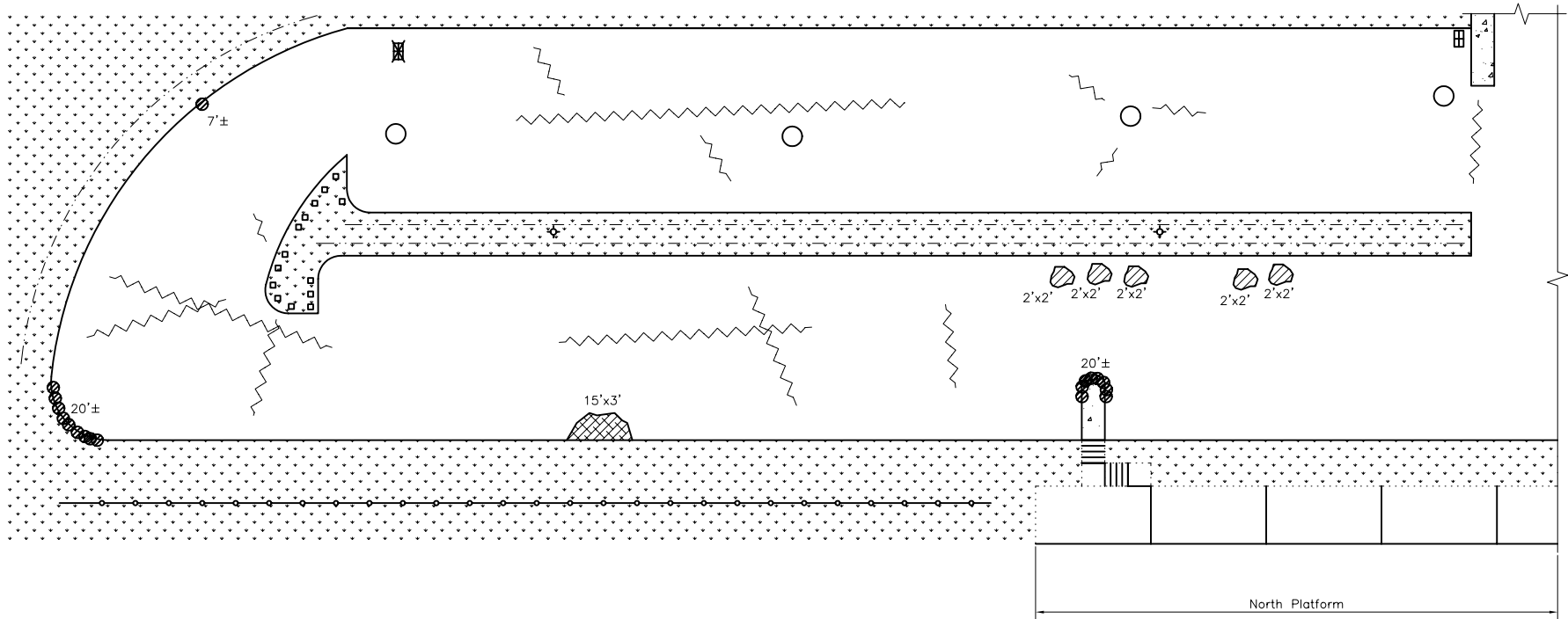
NOTES:

1. Railroad parking is only adjacent to the platform. All other parking is for the adjacent shopping area.

Legend:

- Fence
- Pedestrian Rail
- Crack
- Grass
- Sidewalk
- Drain
- Manhole

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant II Plan
Date: 8/03/02



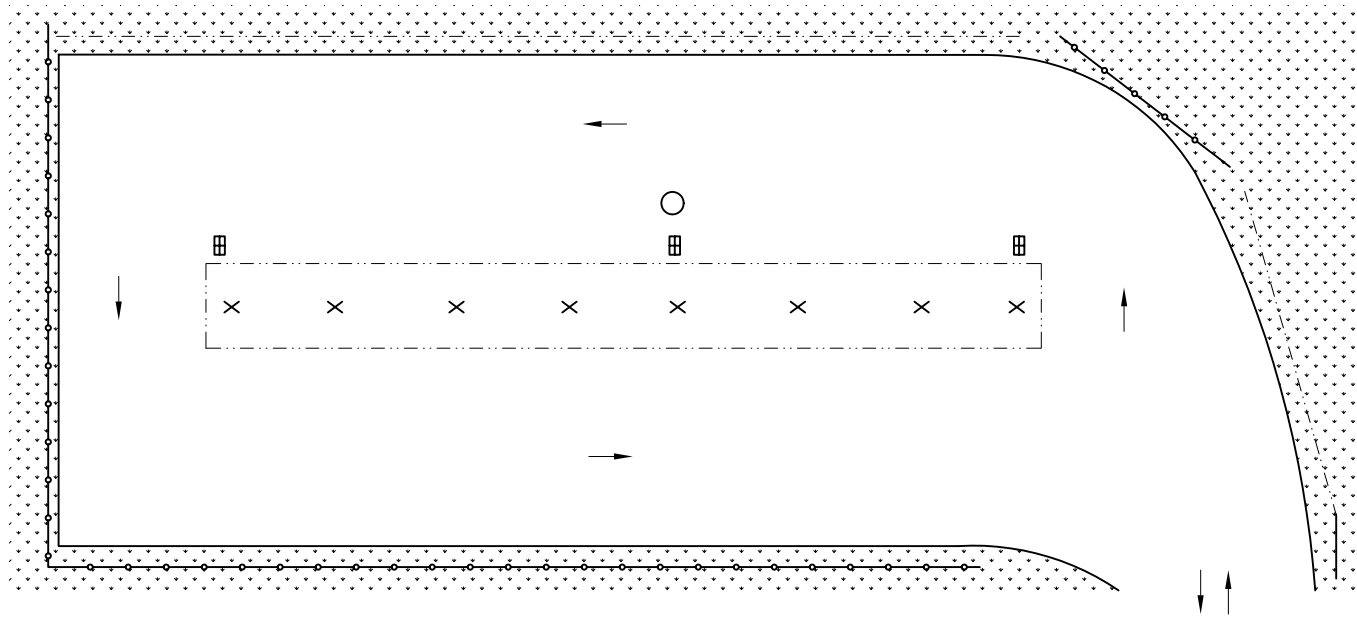
Legend:

- Fence
- Guardrail
- Pedestrian Rail
- Crack
- Ponding of Water
- Uneven Asphalt surface
- Grass
- Sidewalk
- Sanitary Manhole
- Drain
- Drain Covered with Vegetations
- Cracked, Spalled, or Missing Curb
- Timber Post
- Light

NOTES:

1. The asphalt surface is cracked 1000'±.

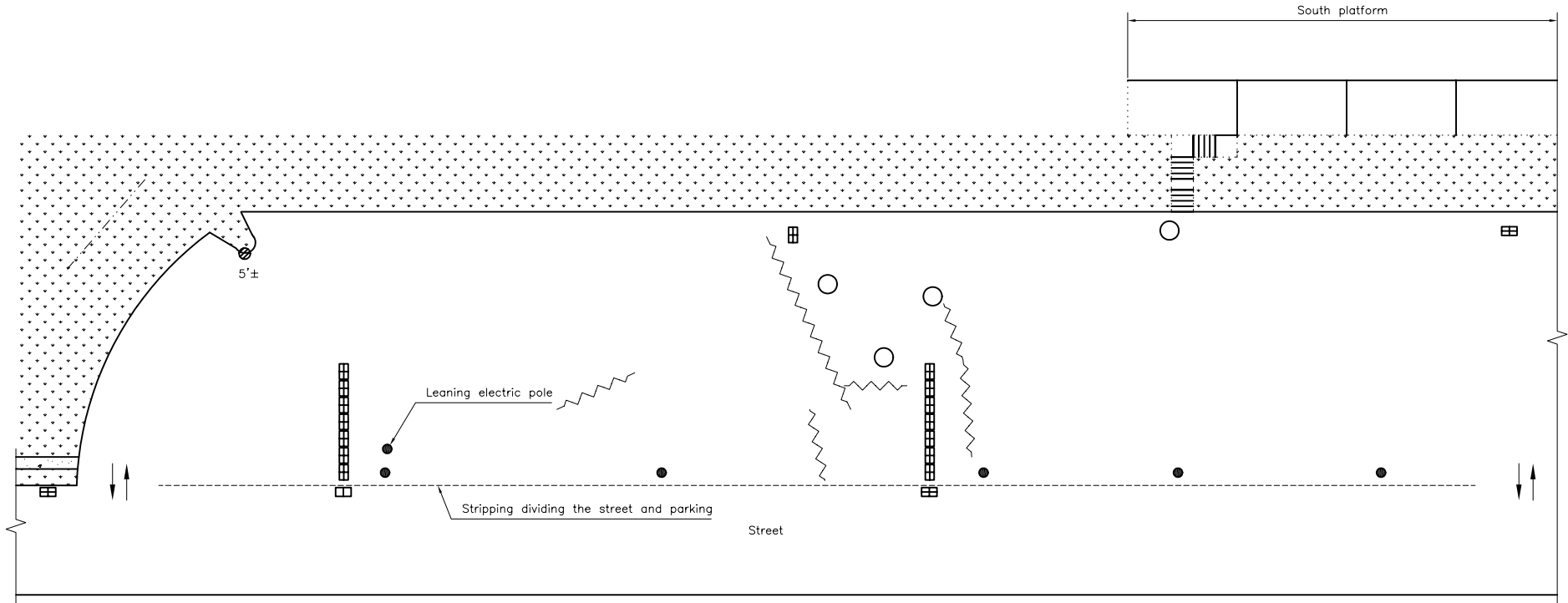
Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant III Plan
Date: 8/03/02



- Legend:
- Fence
  - Guardrail
  - Grass
  - Sanitary Manhole
  - Drain
  - Column for the Roadway Above

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant IV Plan
Date: 8/03/02





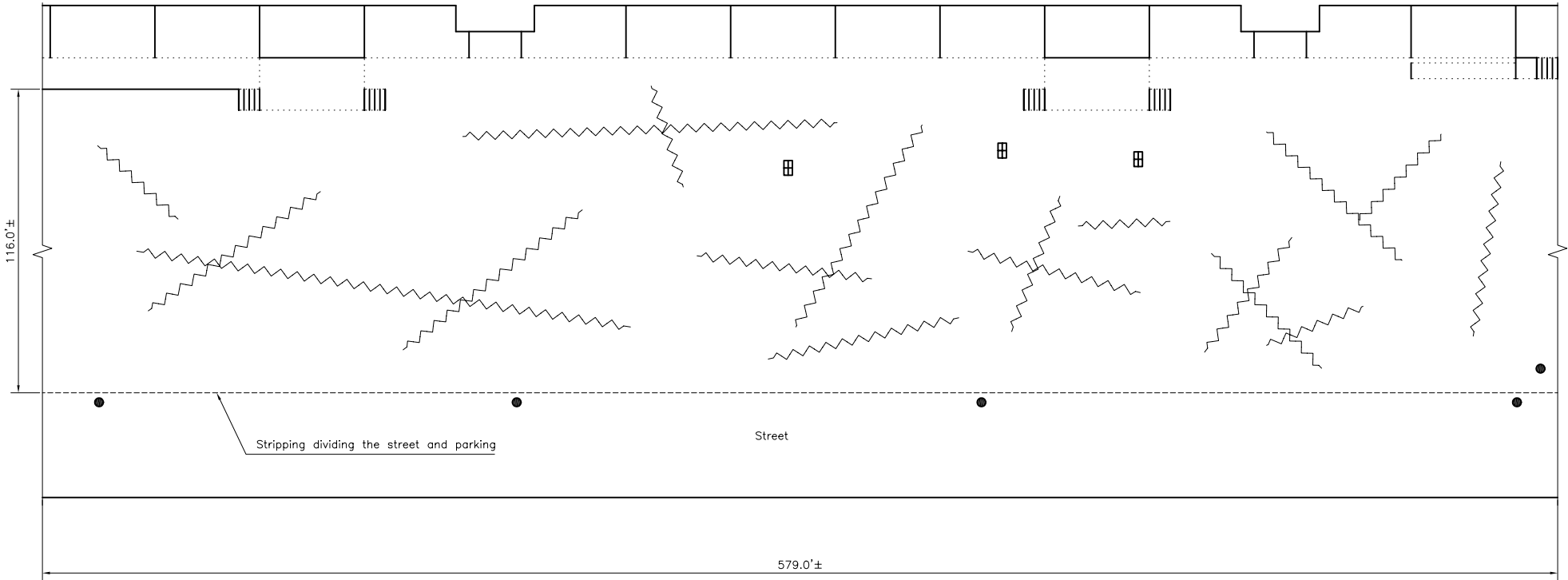
NOTES:

1. The asphalt surface is cracked 200'±.
2. The street and parking are at the same elevation. Stripping divides the two areas.

Legend:

- Guardrail
- ..... Pedestrian Rail
- ~~~~~ Crack
- [Dotted Pattern] Grass
- [Horizontal Lines] Sidewalk
- Sanitary Manhole
- [Grid Pattern] Drain
- ⊗ Cracked, Spalled, or Missing Curb
- Electric Pole

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant V Plan
Date: 8/03/02



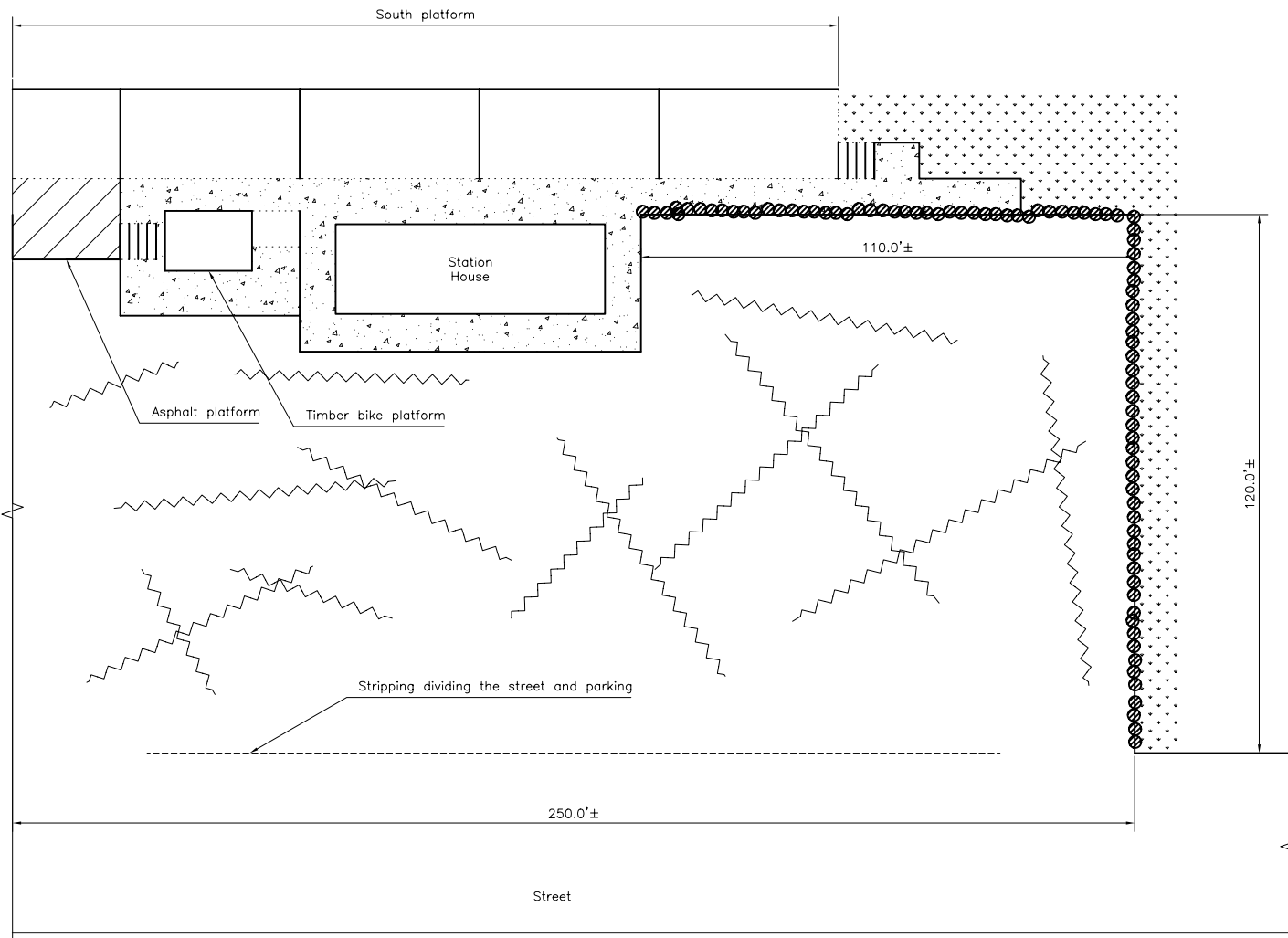
Legend:

- Guardrail
- ..... Pedestrian Rail
- ~~~~~ Crack
- [Stippled Box] Grass
- [Cross-hatched Box] Sidewalk
- [Square with 'X'] Drain
- Electric Pole

NOTES:

1. The entire asphalt surface contains numerous cracks with an uneven driving surface.
2. The street and parking are at the same elevation. Stripping divides the two areas.




Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant VI Plan
Date: 8/03/02



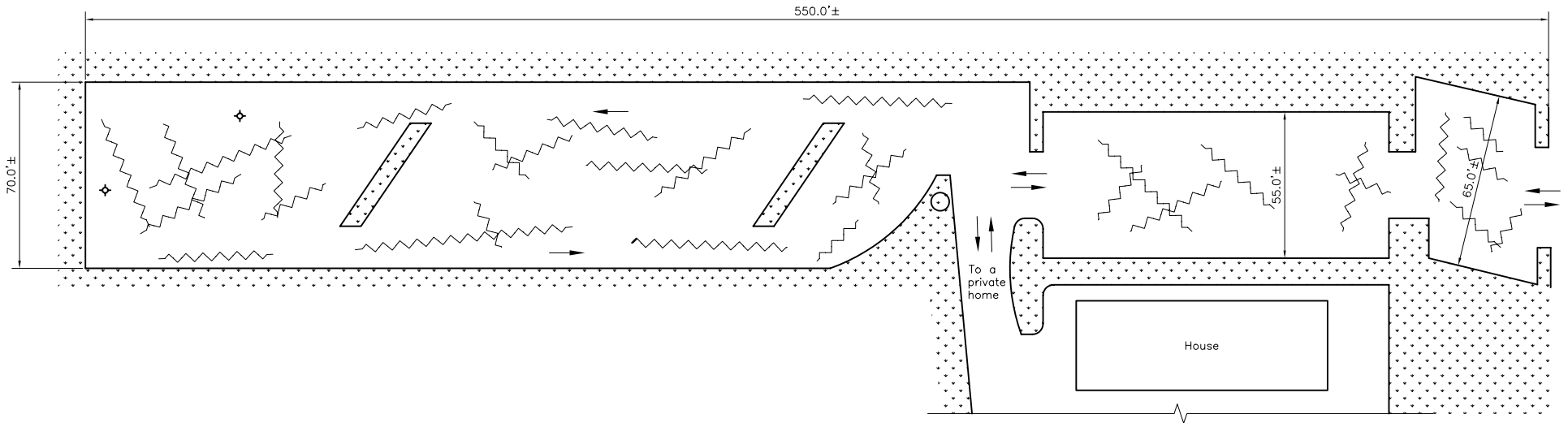
NOTES:

1. The entire asphalt surface contains numerous cracks with an uneven driving surface.
2. This parking area is only for Taxis, Avis car rentals, and LiLi's car rentals.
3. The street and parking are at the same elevation. Stripping divides the two areas.

Legend:

- Pedestrian Rail
- ~~~~~ Crack
-  Grass
-  Sidewalk
-  Cracked, Spalled, or Missing Curb
- Electric Pole

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant VII Plan
Date: 8/03/02



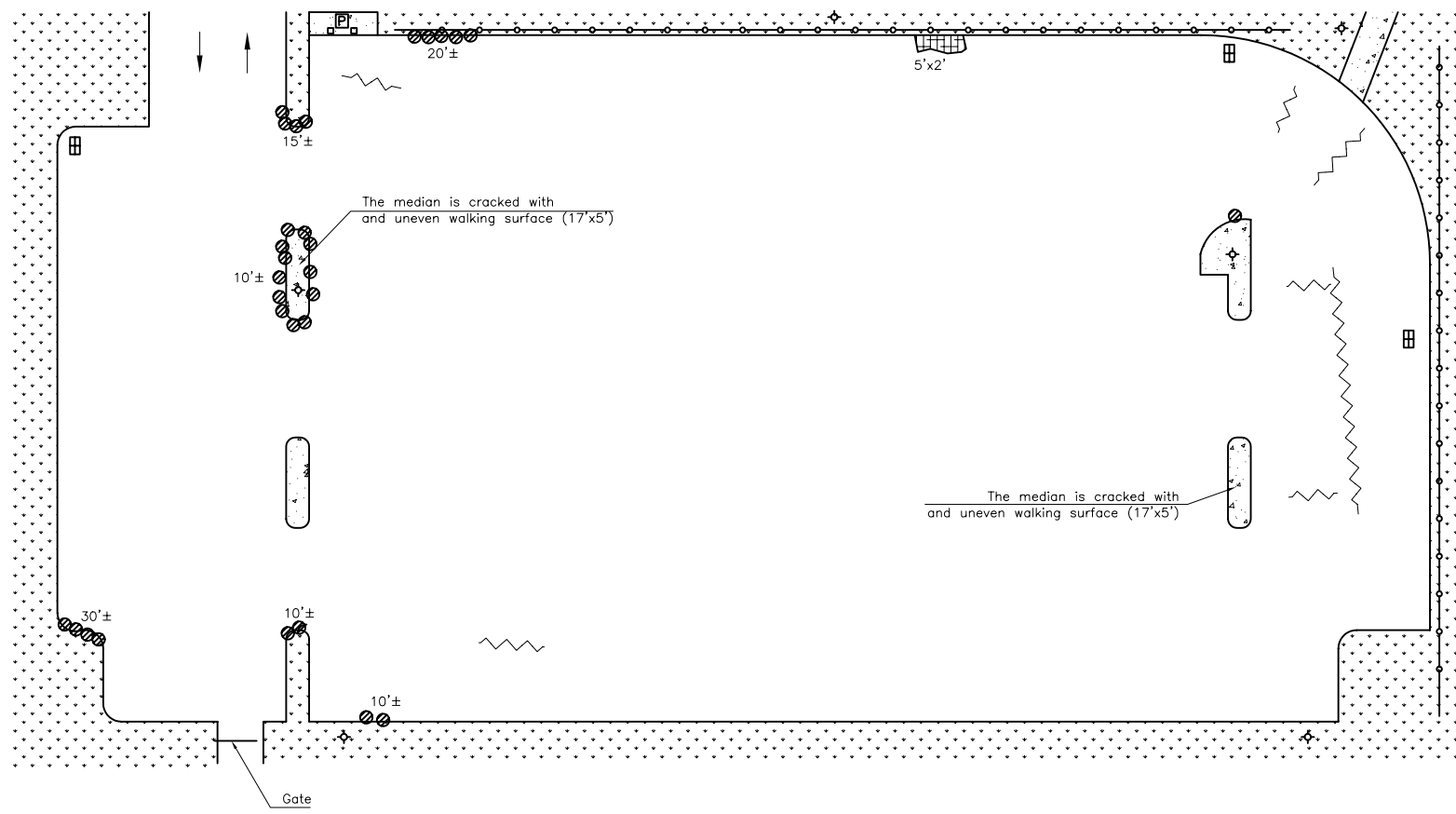
NOTES:

1. The entire asphalt surface contains numerous cracks with vegetation growth and an uneven driving surface.

Legend:

- Crack
- Grass
- Sanitary Manhole
- Light

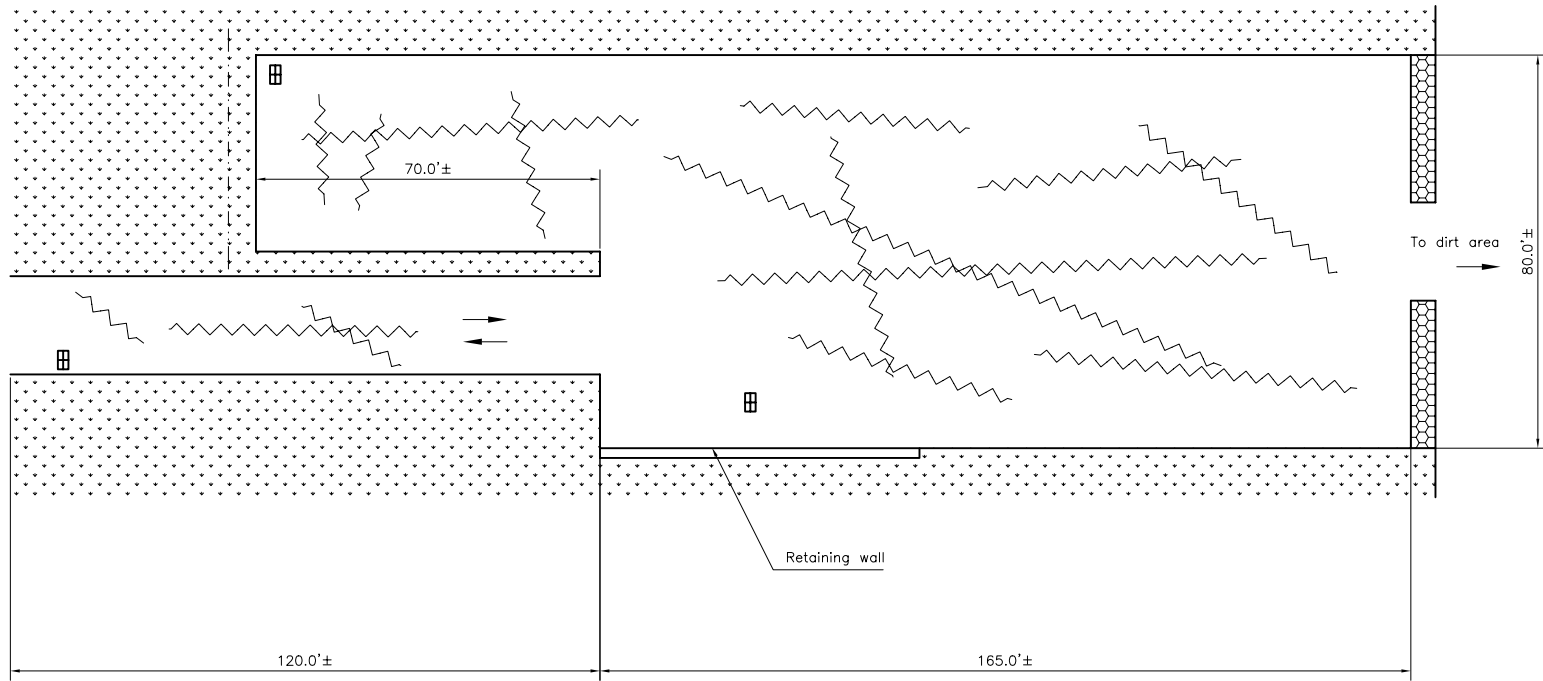
Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant VIII Plan
Date: 8/03/02



- Legend:
- Fence
  - Crack
  - Uneven Driving Surface
  - Grass
  - Sidewalk
  - Drain
  - Cracked, Spalled, or Missing Curb
  - Bollard
  - Light
  - Telephone

NOTES:  
 1. The asphalt surface is cracked 200'±

Urbitran Associates, Inc.
Connecticut Dept. of Transportation
Westport Station Quadrant IX Plan
Date: 8/03/02



Legend:

--- Guardrail

~ Crack

Grass

Concrete Blocks

Drain

NOTES:

1. The entire asphalt surface contains numerous cracks with vegetation growth.

Urbitran Associates, Inc.

Connecticut Dept. of  
Transportation

Westport Station  
Quadrant X Plan

Date: 8/03/02

STATION: Westport

CONN. DEPT OF TRANSPORTATION  
 STATION INSPECTION REPORT  
 SHEET 26 OF 55

INSPECTORS: RGW

DATE: 8/02/02

RATINGS		PHOTO NO.	REMARKS:
NEW	PREV		
2		23	Span VII (N-platform) <b>1</b> - The railing post is cracked, rusted, and deteriorated
2		24	Span I,VI,VII,IX,XI,XII (N-platform) <b>2</b> - The railing base Span I-IV,VII,VIII,X-XXI,XXIII,XXIV (S-platform) plate is rusted.
2		25	Span XIX (S-platform) <b>5</b> - The concrete is spalled and cracked adjacent to the rusted railing base plate
3		33	Span XXII (N-platform) <b>10</b> - There are four isolated areas Span XXI (S-platform) where the slate tile is cracked / chipped
3		26	Span III,IV,V (N-platform) <b>11</b> - The column base plates are rusted and deteriorated
2		27-28	Span III-V, X-XXI (N-platform) <b>14</b> -The canopy paint is chipping Span XIX-XXI (S-platform) and peeling
2		29	Span XX (N-platform) <b>NA</b> - The concrete curb below the railing is cracked
2		30	All spans <b>NA</b> - The base plates for the double tee are rusted and deteriorated
3		31	Span X (S-platform) <b>NA</b> - There are two missing sign/billboard posts
2		32	Span XV (S-platform) <b>NA</b> - The base of the lighting pole is damaged
2		34	Quad I,VI,VII, VIII, X <b>Surface</b> - The asphalt surface contains numerous cracks with vegetation growth
2		35	Quad I,VI,VII, VIII, X <b>Surface</b> - The asphalt surface contains multiple locations of map cracking
2		36	Quad I <b>Surface</b> - Ponding of water was noted in multiple locations





**Westport Station**

Description	Units	Quantity	Price / Unit	Total Cost
<u>Replacing asphalt pavement</u>				
-Remove asphalt	yd <sup>3</sup>	9520.00	\$22.00	\$209,440.00
-6" asphalt top course and binder course	yd <sup>2</sup>	26365.00	\$25.00	\$659,125.00
-7" aggregate base	yd <sup>3</sup>	5125.00	\$20.00	\$102,500.00
<u>Replace sidewalk</u>				
-Remove asphalt	yd <sup>3</sup>	11.00	\$22.00	\$242.00
-6" asphalt top course and binder course	yd <sup>2</sup>	65.00	\$25.00	\$1,625.00
Fill in cracked asphalt	ft	1600.00	\$2.00	\$3,200.00
Remove and install curb	ft	400.00	\$100.00	\$40,000.00
Repair spalled concrete	ft <sup>2</sup>	35.00	\$40.00	\$1,400.00
Replace pedestrian railing	ft	160	\$100.00	\$16,000.00
Clean and paint canopy roof	ft <sup>2</sup>	8400	\$4.00	\$33,600.00
Misc. (clean and paint structural steel )	LS	-	-	\$3,000.00
Misc. (remove veg., slate tile, & etc. )	LS	-	-	\$1,000.00
Replace/repair platform receptacles	EACH	8	\$50.00	\$400.00
Replace platform luminaires *	EACH	26	\$2,800.00	\$72,800.00
<u>Replace platform lighting conduit/conduit fittings</u>				
- fittings	EACH	34	\$75.00	\$2,550.00
- conduit	EACH	1100	\$9.50	\$10,450.00
- type XHHW conductor	EACH	4400	\$0.50	\$2,200.00
Replace platform canopy luminaires *	EACH	8	\$700.00	\$5,600.00
Replace downspout	LS	-	-	\$300.00
Replace lower part of downspout	LS	-	-	\$200.00
Installation of gutter & downspouts southbound platform	LS	-	-	\$5,000.00
Installation of downspouts on northbound rain shelter	LS	-	-	\$3,000.00
Mobilization / Demobilization (10%)				\$117,363.20
Sub-total				\$1,290,995.20
Contingency (20%)				\$258,199.04
Grand Total				\$1,549,194.24
Say				\$1,550,000.00

\* The Quantity of platform luminaires required to bring lighting up to recommended levels is an order-of-magnitude estimate. Performance of a lighting design is required to develop a precise quantity estimate.

# *Lease Narrative and Synopsis*

U R B I T R A N **R** E P O R T



Prepared to

Connecticut Department of Transportation

Submitted by

Urbitran Associates, Inc.

**Urbitran Associates**

**RAILROAD LEASE AGREEMENT  
NARRATIVE**

STATION NAME: **Westport/Saugatuck/Green's Farms**  
STATION OWNER: State of Connecticut Department of Transportation (the "State")  
LESSEE: Town of Westport

Pursuant to the Lease Agreement dated November 27, 1991 (the "Lease"), the State leases to the Town of Westport the land and buildings comprising Saugatuck and Green's Farms Railroad Stations in Westport, for railroad and rail commuter parking purposes. The leased premises consists of ten (10) parcels of land containing an aggregate of 13.015 acres, more or less. The lease term is ten (10) years, commencing on July 1, 1991, to and including June 30, 2001, with the Lessee having the right to renew for two (2) additional successive ten (10) year periods of time. We have assumed that the first 10 year renewal right has been exercised.

There is no annual fee under this Lease. However, Lessee is required to reinvest a portion of the surplus revenue derived from rail parking and rail-related leases into the improvement and maintenance of rail station buildings, rail station parking, and mutually agreed upon Town-allocated expenses. The following expenses are included in Lessee's calculation<sup>1</sup> of the surplus: capital improvements; maintenance of the building and parking lots; administrative, accounting and security costs; utilities; independent auditors; and other mutually agreed upon town-allocated costs, including debt service.<sup>2</sup> The use of one parking space in the Station's "day parking" lot is reserved for the State.

Lessee is required to establish two separate funds: an Operating Fund<sup>3</sup> and a Capital Improvement Fund. Under the Lease, fifty (50) percent of the funds in the Operating Fund, minus operating, maintenance, and annually appropriated capital expenses, must be disbursed to Lessee each year, to be used without limitation. The remaining fifty (50) percent is deposited in the Capital Improvement Fund. The State reserves the right to approve or disapprove the use of funds in the Capital Improvement Fund. Furthermore, fifty (50) percent of the surplus in the Capital Improvement Fund is to be paid to the State every five (5) years.

---

<sup>1</sup> The modified accrual basis of accounting is specified as the method to be used for these records.

<sup>2</sup> Lessee is permitted to include an allocated amount of its debt service as an expense. Only two (2) other leases reviewed expressly provide for debt service to be included in the Town's expenses when calculating the deposit into the Reinvestment Fund.

<sup>3</sup> All revenue generated from the use of the Town-owned and State-leased properties described in the Lease, including all revenue derived from a minimum of 1,665 parking spaces jointly utilized by both parties, must be deposited into the Operating Fund.

The Lease specifies that the State retains sole responsibility for maintaining and/or restoring all fencing bordering the tracks, the canopies over the platforms, tunnels, tunnel drainage, stairways. Furthermore, the Lease contains a standard provision, naming the State as the party solely responsible for all major structural renovations and repairs. The Town retains sole responsibility for the day-to-day maintenance of the leased property, including, but not limited to, any and all platforms, railings, stairs, shelters and ramps (i.e., general structural repairs, snow removal and security).

The Lease is made subject to each and every specification and covenant, unless specifically deleted therefrom, contained in the "Standard Railroad Lease Specifications & Covenants," dated June 1, 1991.

## LEASE SYNOPSIS

<b><u>STATION NAME:</u></b>	<b>Westport/Green's Farms/Saugatuck</b>
<b>Lease Document Reviewed</b>	Lease Agreement dated 11/27/91 (the " <u>Lease</u> ")
<b>Station Owner</b>	State of Connecticut Department of Transportation (the " <u>State</u> ")
<b>Lessee</b>	Town of Westport
<b>Agreement Number</b>	4.16-05(91)
<b>Effective Date of Lease</b>	7/1/91
<b>Term</b>	10 years
<b>Number of Renewal Periods</b>	2 (at Lessee's option)
<b>Renewal Period</b>	10 years each
<b>Number of Lessee Renewals Exercised in Prior Years</b>	1
<b>Number of Renewals Remaining</b>	1
<b>Expiration Date of Original Lease</b>	6/30/2001
<b>Current Expiration Date</b>	6/30/2011
<b>Recorded?</b>	Volume 1133, Page 311
<b>Number of Parcels</b>	10
<b>Total Acreage</b>	13.015 acres
<b>How Is Revenue Earned?</b>	Railroad parking revenue and revenue from railroad-related leases
<b>Are Separate Funds Accounts Required?</b>	Yes. Lessee must establish two separate funds, an Operating Fund and a Capital Improvement Fund. All revenue generated from all sources derived from the use of both Town-owned as well as State-leased properties described in the Lease, including all revenue derived from a minimum of 1,665 parking spaces jointly utilized by the parties to the Lease, must be deposited into the Operating Fund. Funds remaining in the Operating Fund, minus all operating and maintenance expenses, as well as annually appropriated capital expenses, shall be distributed to Lessee on a

	yearly basis at the rate of 50 percent, to be used by Lessee without limitation. The remaining 50 percent of the funds in the Operating Fund shall be deposited in the Capital Improvement Fund.
<b>Allowable Direct Costs in Calculating Surplus</b>	Capital improvements, maintenance of buildings and parking lots, administrative, accounting and security costs, utilities, independent auditors
<b>Allowable Indirect Costs in Calculating Surplus</b>	Yes: mutually agreed upon Town-allocated costs, including debt service
<b>Is Surplus Deposited in Capital Fund?</b>	Yes. Fifty percent of the surplus in the Operating Fund is deposited annually into the Capital Improvement Fund.
<b>Is Surplus Shared with the State?</b>	Yes. The surplus in the Capital Improvement Fund is shared (not the Operating Fund).
<b>How Often is Surplus Shared?</b>	If the Capital Improvement Fund shows a surplus at the end of each 5 year period, the State may elect to withdraw 50 percent of the surplus.
<b>Are Certified Financial Statements Required?</b>	Yes. See <u>Appendix I</u> .
<b>Financial Statement Submission Period</b>	Financial statements are due on January 1 for each year of the Lease term.
<b>Is Annual Budget Required?</b>	No
<b>Is Repayment of Debt Service Required?</b>	No. Lessee is permitted to include an allocated amount of its debt service as an expense under Section 3 of the Lease.
<b>Monthly Debt Repayment Amount</b>	n/a
<b>Does State Pay Lessee a Fee?</b>	No
<b>Amount of Fee Due Lessee</b>	n/a
<b><u>INSURANCE COVERAGE:</u></b>	
<b>Property Damage Insurance</b>	\$750,000.00 individually; \$1,500,000 aggregate
<b>Bodily Injury Coverage</b>	\$750,000.00 individually; \$1,500,000 aggregate
<b>Other Required Coverage</b>	n/a
<b>Voluntary Coverage</b>	n/a
<b>Is Lessee Self Insured?</b>	No

<b>Is Certificate of Coverage on File?</b>	Yes
<b>Named Insured</b>	State of Connecticut and Metro-North
<b>State Held Harmless?</b>	Yes
<b>Lessee Waives Immunity</b>	Yes
<b><u>MAINTENANCE:</u></b>	
<b>State's Responsibilities</b>	The State is responsible for (i) all major structural renovations and/or repairs, and (ii) maintaining and/or restoring all fencing bordering the tracks, canopies over the platforms, the tunnel, tunnel drainage and stairways at the Saugatuck Railroad Station, the stairway from New Creek Road to the platform area on the east and west side of the Green's Farm Railroad Station, and the canopy under the tracks over the sidewalk on New Creek Road at Green's Farm Railroad Station.
<b>Lessee's Responsibilities</b>	Lessee is responsible for day-to-day maintenance, included, but not limited to, any and all platforms, railings, stairs, shelters, and ramps, i.e., general structural repairs, snow removal, and security.
<b>Enhance Aesthetic Appearance</b>	Lessee
<b>Not Erecting Signs on Premises</b>	Lessee
<b>Surface Grade Land</b>	Lessee
<b>Install and Maintain Fencing</b>	Lessee
<b>Install Suitable Drainage</b>	Lessee
<b>Ice Snow Control of Sidewalks</b>	Lessee
<b>Install and Maintain Electrical Systems for Lights</b>	Lessee
<b>Sweeping and Cleaning Litter</b>	Lessee
<b>Station Structures</b>	Lessee
<b>Platform Gutters</b>	Metro-North Commuter Railroad (MNCR)
<b>Fences</b>	Lessee
<b>Signs</b>	Lessee

<b>Platform Lights</b>	MNCR
<b>Drains</b>	State
<b>Equipment</b>	State
<b>Electric and Mechanical Systems</b>	Lessee
<b>Live Rail Facilities</b>	State
<b>Platforms</b>	Lessee
<b>Railings</b>	Lessee
<b>Stairs</b>	Lessee
<b>Platform Shelters</b>	MNCR
<b>Platform Canopy</b>	MNCR
<b>Tunnels</b>	MNCR
<b>Parking Lots</b>	Lessee
<b><u>PARKING:</u></b>	
<b>Total Number of Spaces</b>	Minimum of 1,665
<b>Number of Spaces: State</b>	The State reserves the use of one (1) parking space in the “day parking” lot, on the westbound side of both the Westport Railroad and the Green’s Farm Railroad Station.
<b>Parking Fees</b>	Where there is a charge for parking, the minimum annual fee per vehicle is \$75.00. Any fee in excess of \$75.00 is subject to the State’s approval. Lessee has the right to establish and publish a Daily, Weekly, Annual and/or other periodic Parking-Fee Schedule(s).
<b>Nondiscrimination Clause</b>	See <u>Appendix II</u> .
<b><u>COSTS OF LEASEHOLD:</u></b>	
<b>Taxes Paid by</b>	Lessee
<b>Water</b>	Lessee
<b>Electricity</b>	Lessee



<b>Other Public Utilities</b>	Lessee
<b>Gas</b>	Lessee
<b>Sewer</b>	
<b>Owns Title to Property</b>	State
<b>Owns Title to Capital Improvements</b>	State
<b>Is Subleasing Allowed?</b>	Yes
<b>Can Lease be Sold or Assigned?</b>	No
<b>Is Security Bond Required?</b>	No
<b>If so, the Amount</b>	n/a
<b><u>OTHER:</u></b>	
<b>Termination</b>	The State may terminate this Lease upon one year's notice to Lessee for reasons of default or if the property is needed for transportation related purposes.
<b>Is there a Lease to CT Transit?</b>	No
<b>Employment/Non Discriminatory Requirement</b>	Yes
<b>Miscellaneous</b>	The Lease is made subject to the "Standard Railroad Lease Specifications & Covenants," dated 6/1/91.

# *Station Operations Review*

U R B I T R A N **R** E P O R T



Prepared to  
Connecticut Department of Transportation

Submitted by  
Chance Management

Under Contract to  
Urbitran Associates, Inc.

## WESTPORT

### **Green's Farms and Westport (Saugatuck) Stations**

The Town of Westport has New Haven Line stations, Green's Farms and Westport (Saugatuck) Station. The Town is a stakeholder in the operations of the stations and the administration stated that it prefers to control the way they are managed, operated, and maintained. Since the stations are located within the Town, officials believe that Town control is the most efficient way to operate and manage the stations. Westport residents are heavily invested in the stations and rail service, expecting local officials to provide high quality services across all of the town departments and services, including the stations.

There are plans for major renovations at both stations. At Green's Farms, the surface parking lots are being resurfaced. There are also plans to renovate the interior of the station building. At the Saugatuck station there are plans, with construction now underway as of May, 2003, to renovate the tunnel that connects the east and westbound sides of the tracks. Furthermore, this work will include a renovation of the westbound Saugatuck station. The ADA is the source of funding for these two Saugatuck projects. The Town of Westport has proposed additional changes for the renovations, and the Town will pay one-third of the additional funds for the amended proposal. The Town is also planning a renovation of the eastbound station at Saugatuck, but this will be funded exclusively by Westport.

### **Agreements**

The Green's Farms and Saugatuck Stations are under the control of the Westport Police Department (WPD). WPD has a contract with Penna Construction for day-to-day maintenance.

According to the lease, the Town is responsible for the day-to-day maintenance, however the WPD feels the responsibilities of the Town and Metro-North are not distinguishable, primarily regarding the maintenance of the platforms.

### **Organizational Structure**

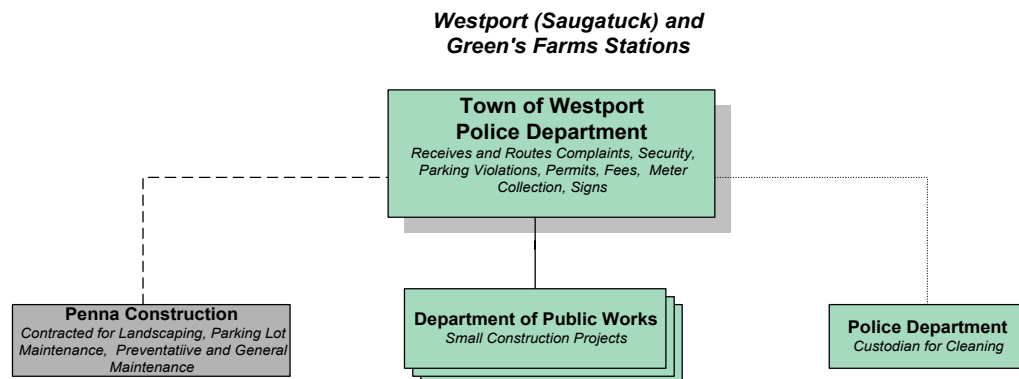
Both Green's Farms and Saugatuck Stations are under the control of the Westport Police Department. William Chiarenzelli is the Police Chief and Alfred Fiore is the Police Captain. Generally, Officer Fiore is the person in charge of the station. He oversees the overall operations of the stations, and William Chiarenzelli oversees the stations from an analytical point of view, helping to recognize problems in the operations of the stations and formulating solutions along with Officer Fiore. Generally, Officer Fiore is

the main contact with the State of Connecticut. Officer Fiore will contact Carl Rosa if there is a concern over something at the station. He indicated that he rarely works with Metro-North because Mr. Rosa will act as the middleman for communication. Further, Officer Fiore is the person who receives complaints or comments from passengers.

The Support Division is the police unit that is delegated to the station operations. While Chief Chiarenzelli and Officer Fiore are responsible for the general management of the stations, there are other people who are involved in the actual work done at the stations. A police officer, Officer Tiberio, is at one of the stations at all times for security reasons. In addition to Officer Tiberio, there are two parking attendants, a part-time custodian, a full-time custodian, and a records clerk. The Police Department is responsible for parking permits, violations, fees, meter collections, and signs.

The Department of Public Works reports directly to the Police Department. Public Works is brought in for small construction projects when necessary. Penna Construction is the primary entity responsible for maintenance and informally reports to WPD.

The Town does not publish an organization chart for the operations and maintenance of the Westport/Saugatuck and Green's Farms Stations and parking lots. The organization chart below was developed from data gathered from Town employees and administrators.



- City Functions
- Other Organizations

## Operating Procedures

The Town is supposed to be responsible for the day-to-day maintenance. Penna Construction is responsible for day to day maintenance through a contract. The Town's Department of Public Works (DPW) performs only small jobs when necessary. If the DPW performs work, then a detailed accounting of the work is given to Officer Fiore by the head of the DPW so that the work can be properly charged to the Railroad Fund. Other work that needs to be performed is contracted as needed.

The Town does not publish any operating procedures. The chart below was developed from information from Town staff and administrators.

<b>Procedure</b>	<b>Responsible Party</b>
Opening and Closing of Station	N/A
Housekeeping Inside Station	Police Department custodian
Housekeeping Outside Station	N/A
Daily Maintenance	Penna Construction, Department of Public Works
Preventative Maintenance	Penna Construction
Landscaping	Penna Construction
Security	Police Department
Customer Service	Police Department
Tenant Performance	Police Department
Parking Enforcement	Police Department
Parking Fees and Permits	Police Department
Parking Operation Maintenance	Penna Construction

# *Station Financial Review*

U R B I T R A N **R** E P O R T



Prepared to  
Connecticut Department of Transportation

Submitted by  
Seward and Monde

Under Contract to  
Urbitran Associates, Inc.

## **WESTPORT / SAUGATUCK & GREEN'S FARMS FINANCES**

### **ACCOUNTING ENTITY / BASIS**

The railroad parking operations at Westport/Saugatuck and Green's Farms are both accounted for in a special revenue fund, the Railroad Parking Fund. The Town of Westport also maintains a special revenue fund for railroad station and parking capital improvements, the Railroad Parking Reserve Fund. Both funds use the modified accrual basis of accounting.

The Town accounts for all (i.e. both Town-owned and State-leased) railroad parking operations in the operating fund and capital improvements are accounted for in the railroad parking reserve fund. According to the lease and as a circumstance to this financial commingling of town-owned and state-leased properties, any profits are shared 50/50. The 50% share that is to be used for capital improvements to the leased railroad properties is to be put into the reserve fund. The Town's 50 % share is to be withdrawn for the Town's use without limitation. In addition, the State can also draw-off 50% of the reserve fund every five-years for use as it deems necessary.

Each year, the surplus is determined and the fifty percent that belongs to the Railroad Parking Reserve Fund is calculated. The Railroad Parking Reserve Fund is to be used for improvements to the station and parking. As previously mentioned, this amount is then required to be moved into the fund and the remaining fifty percent becomes available to the Town for its use. The required transfer, however, is not being made annually. In 1998 there was a catch-up transfer of \$382,988 made from the operating to the capital reserve fund.

### **FINANCIAL REPORTING**

The Town submits its audited general purpose financial statements to the State to report its special revenue funds for railroad parking operations and capital reserve. The financial information submitted is presented in summary format and is not sufficient for financial scrutiny. A separate detailed report is not submitted.

Financial measurements such as unit values for revenues or costs per space, etc. and units further broken down for each lot, is not required by the lease and not included with the financial information.

### **REVENUES**

Revenues consist primarily of permit and daily parking fees. Additional significant revenue is received from rentals and income on invested funds.

Permits are issued through an application process. The issuance and collection of the permit parking revenues is administered by the Westport police department's records

clerk. The clerk also handles the accounting and collection for these fees. The clerk is also responsible for the daily ticketing accounting and uses the Department of Motor Vehicle system to obtain the names of the owners of the parked vehicles.

Daily parking accounting utilizes a tagging system with pre-numbered tickets placed on the windshield of the parked vehicles by lot attendants. The vehicle owners have fourteen days to pay by mail or in person. Late payments are subject to a penalty fee. The lot attendants utilize laptop computers to record the ticket and vehicle information. This information is then downloaded to a database for ticket, vehicle control, and collection control.

## **EXPENSES**

Expenses of the operation consist of payroll (e.g. lot attendants, police, clerk, custodian and supervision). Certain payroll costs are allocated. The calculated allocation is done because the activities of certain persons and departments are not exclusive to the operations of the station. Certain other personnel, such as custodians and lot attendants, who are exclusively involved with the station and parking operations are charged directly.

There is an in-kind service charge by the Town to the operating fund for general and administrative services indirectly provided to the station and parking operations.

*Generally Classified Expenses* – This is mostly office and administrative personnel expense. In 1998, the transfer of \$382,988 to the capital reserve fund was recorded as a generally classified expense.

*Metro-North and ConnDOT* – The State also incurs station expenses through its service agreement with Metro-North / Metropolitan Transit Authority. These expenses are accounted for by Metro-North and included in the charge to the State. The expenses generally relate to maintaining the platform at each station.

The finances of the local government however do not include the station expenses paid by the State to Metro-North under the separate service agreement. These expenses include various maintenance responsibilities related to the stations and especially the platform area. Metro-North performs cyclical maintenance and on-call repairs and maintenance as needed. Metro-North also is responsible to maintain any ticketing area on railroad property. Such costs have been identified and included in the financial presentation.

The Metro-North service agreement also provides that the State pay for the allocated cost of station maintenance forces. These allocated indirect costs have not been included in the financial presentation.

The local government is not in direct control of the services rendered by Metro-North. These services are controlled by the service agreement. The service agreement is outside of the State lease agreement with the local government



ConnDOT also incurs expense for its administrative oversight of the operating leases and the physical properties. These expenses were not compiled or presented in the financial presentation.

### ***EXPENSE ALLOCATION***

The allocation of indirect expenses is a financial issue that would apply to most of the State lease agreements where the local government has determined that administrative charges are warranted and come under the “mutually determined charges” clause of the lease agreements. The lease is not clear as to exactly what charges are allowable. The allocations generally result from common costs such as administrative expenses or departmental expenses that do not exclusively service the railroad properties but service a number of funds and functional activities. The reasonableness or propriety of the allocation and method was not evaluated to determine if such costs were actually incremental or simply attributed to the leased property under a full absorption costing methodology.

### ***PROFITABILITY / ACCUMULATED SURPLUS***

The Town’s operations, excluding Metro-North expenses, generated a losses in both 1996 and 1999. The years 1997, 1998 and 2000, excluding Metro-North expenses, showed profits. At June 30, 2000 the net accumulated surplus totaled \$857,702.

It should be noted that as the result of an audit of the lease, adjustments to the fund balance were made subsequent to the amounts presented in this report and therefore are not reflected in this presentation.

### ***SPECIAL REQUIREMENTS – SURPLUS/RESERVE/DEFICIT***

The lease agreement does not specifically address the administration or funding of any deficit resulting from the State properties managed by the local government.

### ***CAPITAL PROJECTS***

In 1999 there was a major construction project to alleviate the flooding at a lot at Ferry Lane and the lot no. 3 at Saugatuck. This project was financed in part through a state capital grant. At June 30, 2000 the Railroad Parking Reserve Fund was obligated \$100,000 to the Town for temporary financing for the Ferry Lane. The additional state grant reimbursement was due the Town and was subsequently received.

As previously noted, the Town uses a reserve fund to accumulate excess revenues for capital improvement projects. The attached financial presentation excludes the financial activity and fund balances of the reserve fund.

The Railroad Parking Reserve Fund at June 30, 2000 had a balance of \$468,126, reflecting grant proceeds, investment income, the 1998 transfer (\$382,988) from the Railroad Parking Fund and other income, net of capital and other expenditures.

### ***FINANCIAL PRESENTATION IN COMPARISON TO THE PARKING INVENTORY***

A parking inventory and utilization report is presented separately as Task 2 in this study. The financial presentation herein and parking inventory cover all rail parking spaces for the two stations.

The parking inventory covers State-leased and municipal rail parking spaces at both the Westport/Saugatuck and Green's Farms stations. State spaces total about 1590 and represent nearly 83% of the available spaces. Municipal spaces include 100 park and ride spaces from which shuttle services is provided to the Saugatuck station.

**WESTPORT/SAUGATUCK & GREEN'S FARMS RAILROAD STATION AND PARKING OPERATIONS**

	YEAR 1996				YEAR 1997			
	OPERATING AGREEMENTS				OPERATING AGREEMENTS			
	LOCAL GOVT	METRO-NORTH	TOTAL	%	LOCAL GOVT	METRO-NORTH	TOTAL	%
<b>REVENUES</b>								
PARKING	\$ 561,483	\$ -	\$ 561,483	89.4%	\$ 760,404	\$ -	\$ 760,404	90.2%
RENTS	44,316	-	44,316	7.1%	47,414	-	47,414	5.6%
INVESTED FUNDS	15,502	-	15,502	2.5%	23,180	-	23,180	2.7%
OTHER	7,015	-	7,015	1.1%	12,240	-	12,240	1.5%
	<u>\$ 628,316</u>	<u>\$ -</u>	<u>\$ 628,316</u>	<u>100.0%</u>	<u>\$ 843,238</u>	<u>\$ -</u>	<u>\$ 843,238</u>	<u>100.0%</u>
 <b>STATION, PLATFORMS AND PARKING EXPENSES</b>								
REPAIRS AND MAINTENANCE	\$ 177,985	\$ 55,436	\$ 233,421	31.3%	\$ 90,857	\$ 51,303	\$ 142,160	21.1%
UTILITIES	40,281	-	40,281	5.4%	40,346	-	40,346	6.0%
RENT	-	-	-	0.0%	-	-	-	0.0%
SECURITY	-	-	-	0.0%	-	-	-	0.0%
INSURANCE AND CLAIMS	54,243	700	54,943	7.4%	55,819	18	55,837	8.3%
GENERALLY CLASSIFIED EXPENSES (INCLUDING UNSPECIFIED -DIRECT, -INDIRECT, -ADMINISTRATIVE, - AND GENERAL ALLOCATIONS)	371,596	11,671	383,267	51.4%	367,806	15,051	382,858	56.7%
CONNECTICUT SALES TAX	33,363	-	33,363	4.5%	53,712	-	53,712	8.0%
	<u>\$ 677,469</u>	<u>\$ 67,807</u>	<u>\$ 745,276</u>	<u>100.0%</u>	<u>\$ 608,540</u>	<u>\$ 66,372</u>	<u>\$ 674,912</u>	<u>100.0%</u>
 <b>NET PROFIT (LOSS)</b>	<u>\$ (49,153)</u>	<u>\$ (67,807)</u>	<u>\$ (116,960)</u>		<u>\$ 234,698</u>	<u>\$ (66,372)</u>	<u>\$ 168,326</u>	
 <b>LOCAL GOVERNMENT'S RAILROAD FUND</b>								
ACCUMULATED SURPLUS	\$ 248,371				\$ 483,069			
LESS- LOCAL GOVERNMENT'S SHARE	(124,186)				(241,535)			
NET AVAILABLE RAILROAD FUND SURPLUS	<u>\$ 124,186</u>				<u>\$ 241,535</u>			
 <b>STATE'S AVAILABLE SHARE @ 50%</b>	<u>\$ 62,093</u>				<u>\$ 120,767</u>			

**WESTPORT/SAUGATUCK & GREEN'S FARMS RAILROAD STATION AND PARKING OPERATIONS**

<b>REVENUES</b>	<b>YEAR 1998</b>				<b>YEAR 1999</b>			
	<b>OPERATING AGREEMENTS</b>				<b>OPERATING AGREEMENTS</b>			
	<b>LOCAL GOVT</b>	<b>METRO-NORTH</b>	<b>TOTAL</b>	<b>%</b>	<b>LOCAL GOVT</b>	<b>METRO-NORTH</b>	<b>TOTAL</b>	<b>%</b>
PARKING	\$ 803,203	\$ -	\$ 803,203	89.3%	\$ 787,241	\$ -	\$ 787,241	89.6%
RENTS	52,479	-	52,479	5.8%	54,568	-	54,568	6.2%
INVESTED FUNDS	39,902	-	39,902	4.4%	36,613	-	36,613	4.2%
OTHER	4,307	-	4,307	0.5%	466	-	466	0.1%
	<u>\$ 899,891</u>	<u>\$ -</u>	<u>\$ 899,891</u>	<u>100.0%</u>	<u>\$ 878,888</u>	<u>\$ -</u>	<u>\$ 878,888</u>	<u>100.0%</u>
<b>STATION PLATFORMS AND PARKING EXPENSES</b>								
REPAIRS AND MAINTENANCE	\$ 79,572	\$ 52,130	\$ 131,702	12.4%	\$ 112,047	\$ 54,910	\$ 166,957	23.3%
UTILITIES	37,363	-	37,363	3.5%	34,933	-	34,933	4.9%
RENT	-	-	-	0.0%	-	-	-	0.0%
SECURITY	-	-	-	0.0%	-	-	-	0.0%
INSURANCE AND CLAIMS	43,752	6,000	49,752	4.7%	59,629	8	59,637	8.3%
GENERALLY CLASSIFIED EXPENSES (INCLUDING UNSPECIFIED -DIRECT, -INDIRECT, -ADMINISTRATIVE, - AND GENERAL ALLOCATIONS)	780,543	13,146	793,689	74.5%	B 397,507	13,294	410,801	57.2%
CONNECTICUT SALES TAX	52,456	-	52,456	4.9%	45,382	-	45,382	6.3%
	<u>\$ 993,687</u>	<u>\$ 71,276</u>	<u>\$ 1,064,963</u>	<u>100.0%</u>	<u>\$ 649,497</u>	<u>\$ 68,212</u>	<u>\$ 717,709</u>	<u>100.0%</u>
<b>NET PROFIT (LOSS)</b>	<u>\$ (93,796)</u>	<u>\$ (71,276)</u>	<u>\$ (165,072)</u>		<u>\$ 229,391</u>	<u>\$ (68,212)</u>	<u>\$ 161,179</u>	
<b>LOCAL GOVERNMENT'S RAILROAD FUND</b>								
ACCUMULATED SURPLUS	\$ 389,273				\$ 618,664			
LESS- LOCAL GOVERNMENT'S SHARE	(194,637)				(309,332)			
NET AVAILABLE RAILROAD FUND SURPLUS	<u>\$ 194,637</u>				<u>\$ 309,332</u>			
<b>STATE'S AVAILABLE SHARE @ 50%</b>	<u>\$ 97,318</u>				<u>\$ 154,666</u>			

**WESTPORT/SAUGATUCK & GREEN'S FARMS RAILROAD STATION AND PARKING OPERATIONS**

	YEAR 2000			
	OPERATING AGREEMENTS			
	LOCAL GOVT	METRO-NORTH	TOTAL	%
<b>REVENUES</b>				
PARKING	\$ 882,038	\$ -	\$ 882,038	94.4%
RENTS	-	-	-	0.0%
INVESTED FUNDS	51,946	-	51,946	5.6%
OTHER	-	-	-	0.0%
	<u>\$ 933,984</u>	<u>\$ -</u>	<u>\$ 933,984</u>	<u>100.0%</u>

**NOTES....**

- A** = 1996 to 1997 Snow removal decreased by \$83,600
- B** = 1998 Includes \$382,988 Transferred to Reserve Fund

**STATION PLATFORMS AND PARKING EXPENSES**

REPAIRS AND MAINTENANCE	\$ 692,632	\$ 64,712	\$ 757,344	86.6%
UTILITIES	-	-	-	0.0%
RENT	-	-	-	0.0%
SECURITY	-	-	-	0.0%
INSURANCE AND CLAIMS	-	17,700	17,700	2.0%
GENERALLY CLASSIFIED EXPENSES (INCLUDING UNSPECIFIED -DIRECT, -INDIRECT, -ADMINISTRATIVE, -AND GENERAL ALLOCATIONS)	80,865	18,992	99,857	11.4%
CONNECTICUT SALES TAX	-	-	-	0.0%
	<u>\$ 773,497</u>	<u>\$ 101,404</u>	<u>\$ 874,901</u>	<u>100.0%</u>

**NET PROFIT (LOSS)**

\$ 160,487    \$ (101,404)    \$ 59,083

**LOCAL GOVERNMENT'S RAILROAD FUND**

ACCUMULATED SURPLUS	\$ 779,151
LESS- LOCAL GOVERNMENT'S SHARE	(389,576)
<b>NET AVAILABLE RAILROAD FUND SURPLUS</b>	<u>\$ 389,576</u>

**STATE'S AVAILABLE SHARE @ 50%**

\$ 194,788

Traffic and Transportation  
Bridge and Civil Engineering  
Architecture  
Parking Services  
Construction Inspection  
Environmental Services  
Transit Services  
Structural Engineering

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