



**Connecticut
Light & Power**

A Northeast Utilities Company

Stamford Reliability Cable Project

Connecticut Siting Council

Public Field Review

March 28, 2013

1:00 p.m.



Field Review Guides:

Peter Novak, Senior Engineer, CL&P Transmission

Chris Swan, Director, Municipal Relations and Siting

Please note:

- Today we'll be reviewing the Preferred Route, the Preferred Route With Variation and the Alternate Route. In addition, we'll review the Preferred Route With Canal Street Option.
- To ensure our safety, tour segments along the Preferred Route have been renumbered to be in sync with one way traffic patterns.

Safety First and Always



- We will have a police escort to ensure proper traffic flow.
- Please remain seated while the vehicle is in motion.
- Watch your step exiting and entering the vehicle.
- A stop at the Glenbrook Substation midway through the review is planned for your convenience, if necessary.

Project Overview



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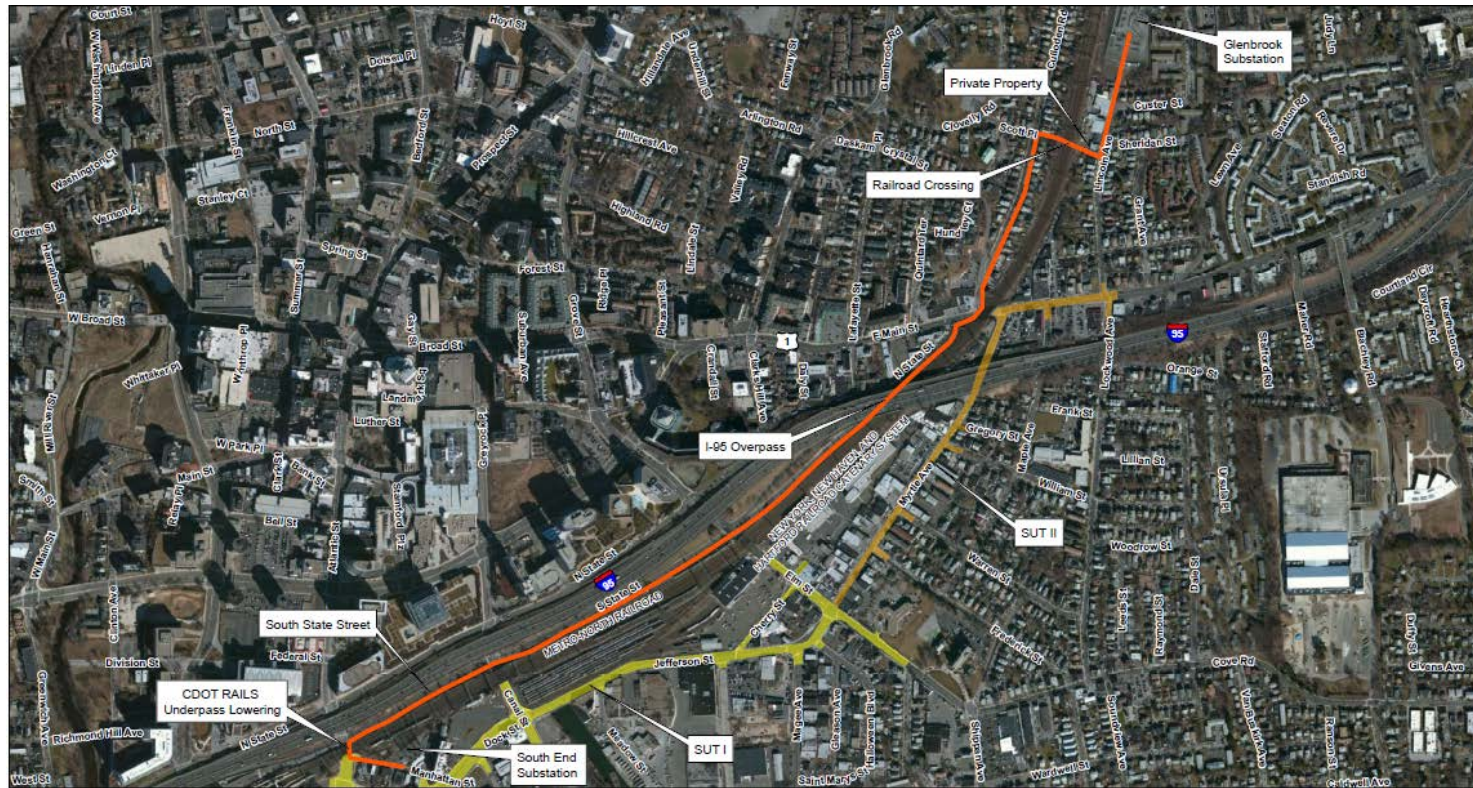


-  Preferred Route
-  Preferred Route With Variation
-  Alternate Route
-  Substation
-  Railroad

Preferred Route



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At 8,000 feet in length, the Preferred Route is the shortest of the three proposed routes. It is located primarily along city streets and would require crossing the Metro-North Railroad (MNR) corridor. It would also require two easements – one on private property, and one from the City of Stamford.

South End Substation to Manhattan St. (One Way) (CSC application segment 7 – 350 feet)



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Beginning at CL&P's South End Substation, the route extends westerly 350 feet, along Manhattan Street



South State St. to North State St. (CSC application segment 5 - 975 feet)



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The route continues northeasterly on South State Street, then merges onto North State Street and crosses under the elevated I-95 roadway

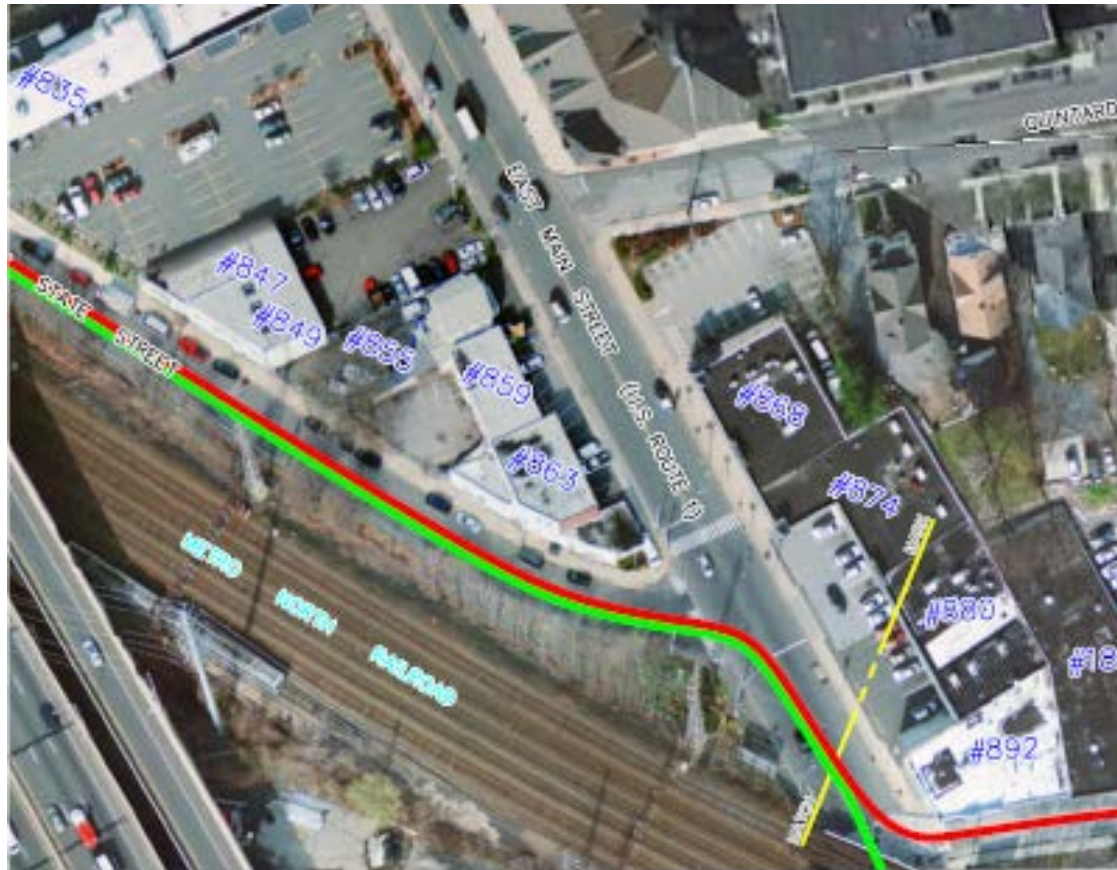


North Main St. to East Main St./Route 1 (CSC application segment 4 - 175 Feet)



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The route continues northeasterly on North State Street to East Main Street/Route 1



East Main St./Route 1 to Culloden Rd./Crystal St. (CSC application segment 3 - 1,230 feet)



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The route crosses East Main Street/Route 1, turns northerly up Crystal Street, and veers right onto Culloden Road



Culloden Rd. to Scott Place (CSC application segment 2 - 480 feet)



The route continues northerly along Culloden Road to the Scott Place intersection, where it turns easterly on Scott Place and then crosses the MNRR corridor. The MNRR crossing is approximately 140 feet.



Lincoln Ave. to CL&P's Glenbrook Substation (CSC application segment 1 - 735 feet)



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The cable extends across the private property, then merges onto Lincoln Avenue. The cable route finally extends northerly, up Lincoln Ave. and ending at the CL&P Glenbrook Substation.



Preferred Route With Variation



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The variation differs from the Preferred Route in that it does not require crossing the MNRR corridor or a private property easement. However, it is marginally longer (8,080 feet) and will require an encroachment agreement with ConnDOT due to a longer section of the route along East Main Street/Route 1. This route also affects the City of Stamford's SUT Project area.

Glenbrook Substation to Lincoln Ave. to East Main St./Rt. 1 (CSC application segment 1 - 1,650 feet)



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Originating at the CL&P Glenbrook Substation, the route extends down the entire length of Lincoln Ave., to the intersection with East Main St./Route 1



East Main St./Route 1 to North State St. (CSC application segment 2 - 1,050 feet)



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The route then turns westerly on East Main St./Route 1 (part of SUT II project) and extends through the MNRR underpass to North State St.



North State St. to South State St. (CSC application segment 3 - 975 feet)



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The route then continues southwesterly along North State Street, then bears left onto South State Street, and crosses under the elevated I-95 roadway



South State St. to Atlantic St. to Manhattan St. (CSC application segment 4 - 4,055 feet)



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The route continues southwesterly on South State Street to Atlantic Street where it turns southeasterly onto Atlantic Street, crossing through the MNRR corridor underpass to Manhattan Street



Manhattan Street to South End Substation

(CSC application segment 5 - 350 feet)



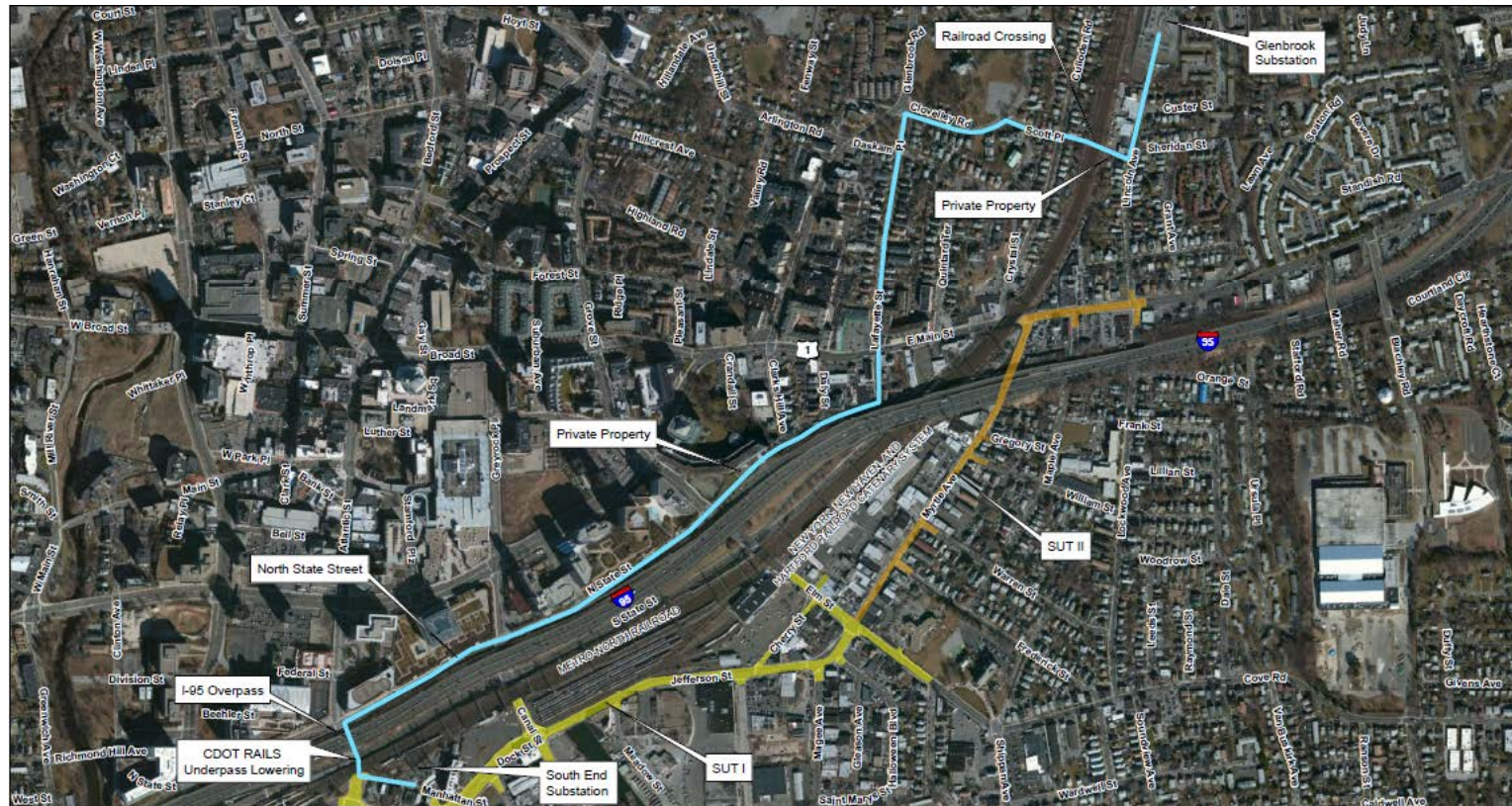
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The route extends easterly along Manhattan Street terminating in the CL&P South End Substation





Alternate Route



At 8,800 feet in length, the Alternate Route is the longest of the three routes. As with the Preferred Route, it requires crossing the MNRRC corridor. In addition, this route would require easements on two private properties.

Glenbrook Substation to Lincoln Ave. to Private Property

(CSC application segment 1 - 735 feet)



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Originating at CL&P's Glenbrook Substation, the cable route extends southerly, down Lincoln Avenue, to a location past Sheridan Street where it turns westerly onto private property



Scott Place to Clovelly Rd. (CSC application segment 2 – 830 feet)



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The route continues westerly across the MNRR corridor (140-foot crossing), connecting to Scott Place.

The route continues on Scott Place, crossing through the Culloden Road intersection, and extending westerly, to the Clovelly Road intersection



*Clovelly Rd. to Lafayette St.
(CSC application segment 3 - 670 feet)*



The route then extends westerly down Clovelly Road to the intersection with Lafayette Street



Lafayette St. to East Main St./Rt. 1 to North State St. (CSC application segment 4 - 1,880 feet)



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The route then turns southerly down Lafayette Street crossing over East Main Street/Route 1, connecting to North State Street

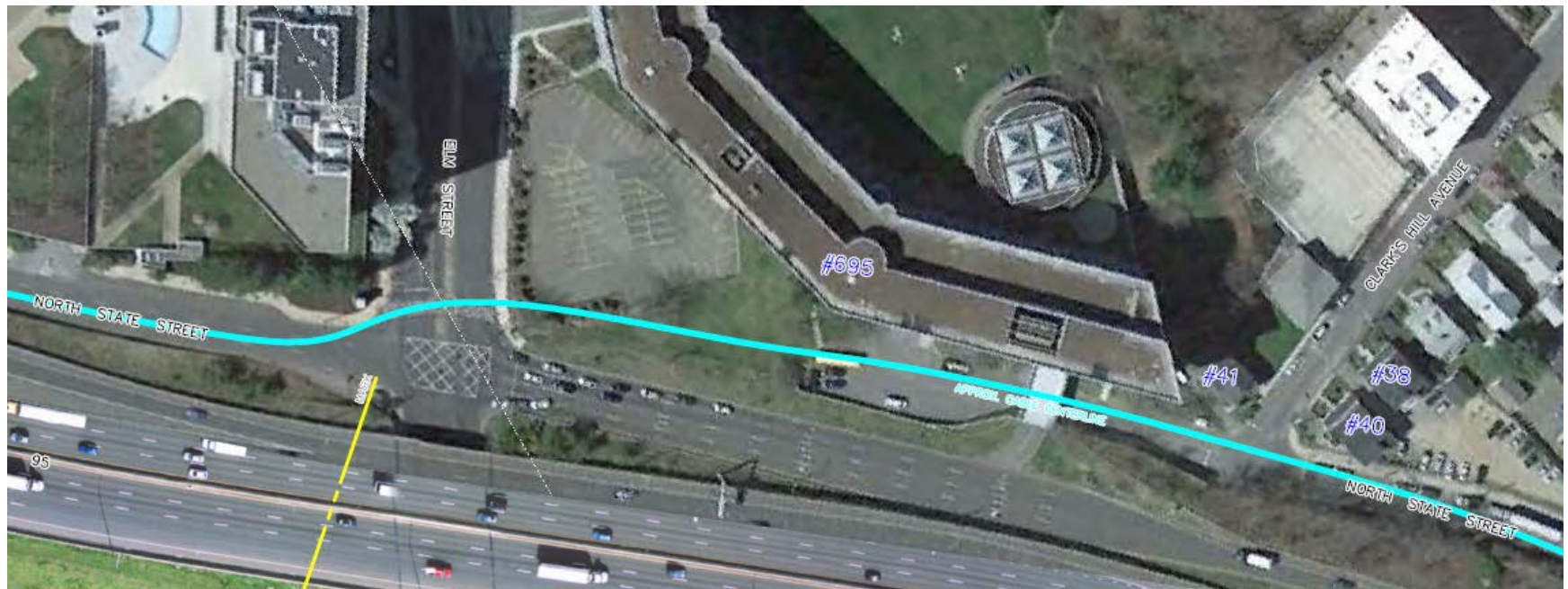


*North State St. to Clarks Hill Ave. to Elm St.
to Atlantic Ave.
(CSC application segment 5 - 4,030 feet)*



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The route then extends southwesterly along North State Street, just north of the I-95 corridor to Clarks Hill Avenue, where it enters the rear loading area access road of the Financial Centre. The route passes along this access road, through a grassy area to Elm Street, and then merges back onto North State Street. The route then continues southwesterly along North State Street to Atlantic Street



*North State St. to Atlantic St. to Manhattan St.
(CSC application segment 6 - 305 feet)*



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The route then turns southeasterly onto Atlantic Street, passing through the I-95 underpass and the MNRR corridor underpass to Manhattan Street



Manhattan St. to South End Substation (CSC application segment 7 - 350 feet)



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The route then extends easterly along Manhattan Street, ending at CL&P's South End Substation



Preferred Route With Canal Street Option



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